



# City of Fairfax, Virginia

## City Council Work Session

Agenda Item # 12a

City Council Meeting 6/25/2024

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TO: Honorable Mayor and Members of the City Council

FROM: Laszlo A. Palko, City Manager *Laszlo A. Palko*

SUBJECT: Discussion on a request of ABTB Mid-Atlantic, LLC to amend the General Development Plan (proffer plan amendment) from retail to a restaurant with drive-through at the Boulevard Marketplace Shopping Center (10140 Fairfax Boulevard)

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ISSUE(S): The applicant, ABTB Mid-Atlantic, LLC, proposes a Zoning and General Development Plan Amendment for Boulevard Marketplace Shopping Center (10140 Fairfax Boulevard) in the Commercial Retail District and Architectural Control Overlay District (ACOD); a Special Use Permit to allow a restaurant with a drive-through facility (10120 Fairfax Boulevard), and a Major Certificate of Appropriateness for architecture and landscaping.

SUMMARY: City Council Work Session to replace an approved 5,100 square foot commercial retail building with a 2,256 square foot restaurant with a drive-through facility on 0.66 +/- acres. The general development plan shows a drive-through facility that wraps around a one-story 2,256 square foot building and parking lot area for customers. The applicant has provided nine (9) spaces adjacent to the entrance of the east side of the building and twelve (12) spaces that would be shared with other uses in the shopping center for a total of 21 spaces. The proposed drive-through facility has 13 stacking spaces from the drive-through window on the west side of the building and nine (9) spaces from the order station. The applicant has proposed a 190 square foot outdoor patio seating area near the main entrance to the restaurant.

FISCAL IMPACT: The fiscal impact analysis is under review.

RECOMMENDATION: Discussion on proposed redevelopment

#### ALTERNATIVE

COURSE OF ACTION: The City Council may choose not to conduct the discussion or defer the discussion to a future date.

STAFF CONTACT: Albert Frederick, Senior Planner; Jason Sutphin, Community Development Division Chief; Brooke Hardin, Director, Community Development & Planning

COORDINATION: Community Development & Planning                      Public Works  
Code Administration/Fire    Police  
Historic Resources    Real Estate

ATTACHMENTS: Staff Report, Statement of Justification, General Development Plan Amendment, Traffic Impact Study, BAR Work Session Staff Report



# CITY OF FAIRFAX

## Department of Community Development & Planning

General Development Plan Amendment (Z-24-00085)  
Special Use Permit (SU-24-00086)

### WORK SESSION DATE

June 25, 2024

### APPLICANT

ABTB Mid-Atlantic, LLC

### AGENT

Kathryn Taylor, Attorney  
Walsh Colucci Lubeley & Walsh  
2200 Clarendon Boulevard  
Suite 1300  
Arlington, VA 22201

### APPLICATION SUMMARY

The intent of this post-submission work session request is to receive feedback from the City Council on an amendment to the Zoning and General Development Plan for Boulevard Marketplace Shopping Center in the CR Commercial Retail District and Architectural Control Overlay (ACOD); and a Special Use Permit to allow a restaurant with a drive-through facility. This land use application is subject to a Major Certificate of Appropriateness for architecture and landscaping. The applicant proposes to develop a vacant pad site that is part of the Boulevard Shopping Center to a 2,256 square foot restaurant with a drive-through on approximately 0.66 +/- acres.

### PARCEL DATA

#### Tax Map ID

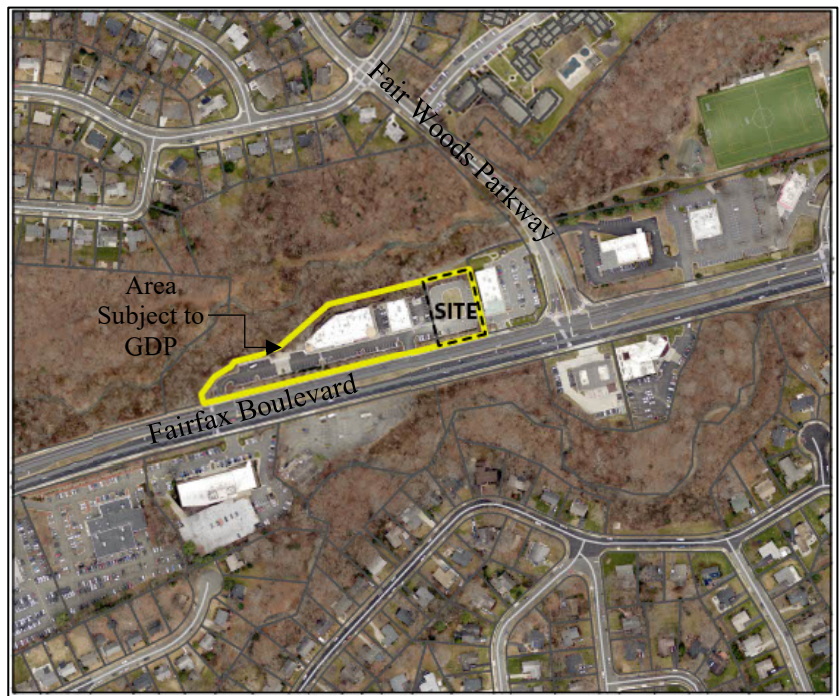
- ◇ 47-4-23-000-A
- ◇ 47-4-23-000-D

#### Street Address

- ◇ 10140 Fairfax Boulevard
- ◇ 10120 Fairfax Boulevard

#### Zoning District

- ◇ CR Commercial Retail
- ◇ Architectural Control Overlay District (ACOD)



Background Information

The applicant, ABTB Mid-Atlantic, LLC, is the contract lessee of a 0.66-acre parcel (Parcel D) in the Boulevard Marketplace Shopping Center. The subject property is located on the north side of Fairfax Boulevard and approximately 250 west of Fair Woods Parkway and adjacent to Patient First Primary and Urgent Care - Fairfax. The primary uses surrounding the shopping center are commercial in nature except for Fairfax City Fire Station 33 on the south side of Fairfax Boulevard. To the north and west of the site is floodplain and the Chesapeake Bay Resource Protection Area (RPA). Table 1 (below) provides the site data for the subject property.

Table 1: Site Data

Address	Parcel ID	Owner	Acres
10140 Fairfax Boulevard	47-4-23-000-A	SCC Boulevard Marketplace LLC	2.48
10120 Fairfax Boulevard	47-4-23-000-D	JDC Boulevard, LLC c/o Rosenthal Properties	0.66
Total			3.14

- 10140 Fairfax Boulevard: 19,926 square foot shopping center with 112 parking spaces was constructed in 2012.
- 10120 Fairfax Boulevard: Undeveloped parcel with an approved 5,100 square foot retail building. Currently, sits as a pad site for a future end user.

The primary access to the subject property is through the existing shopping center. The existing shopping center is served by two access points to Fairfax Boulevard. A secondary inter-parcel access point is provided through the Patient First site with access to Fairfax Boulevard and to Fair Woods Parkway. The overall shopping center also has infrastructure improvements, such as an existing storm drain structure, sanitary sewer line, water line, signs, streetlights and parking lot lights, traffic poles and a pedestrian path that connects to Fair Woods Parkway. The frontage along Fairfax Boulevard has an 8-foot sidewalk. Further information on adjacent properties is provided in Table 2 (below):

Table 2: Surrounding Land Use and Zoning

	Existing Zoning	Existing Land Use	Future Land Use
Site	CR Commercial Retail/Architectural Control Overlay District (ACOD)	Undeveloped parcel Shopping Center	Commercial Corridor
North	CR Commercial Retail/Architectural Control Overlay District (ACOD) Light Industrial/Architectural Control Overlay District (ACOD)	Undeveloped land	Green Network-Public
South	CR Commercial Retail/Architectural Control Overlay District (ACOD)	Fire Station, offices, and restaurant	Commercial Corridor Green Network-Public Social and Civic Network
East	CR Commercial Retail/Architectural Control Overlay District (ACOD)	Patient First	Commercial Corridor
West	CR Commercial Retail/Architectural Control Overlay District (ACOD)	Undeveloped land	Commercial Corridor

The Future Land Use designation for the subject property is Commercial Corridor. The surrounding land use designation is Commercial Corridor Place Type to the east and west, while the future land use designation to

the north is Green Network - Public and a mixture of Commercial Corridor, Green Network – Public, and Social and Civic Network.

### Project History

On February 27, 2023, the Planning Commission held a pre-application briefing to discuss the proposed 2,256-square foot restaurant with drive-through window/facility located in the Boulevard Marketplace Shopping Center. The Planning Commission expressed the following concerns with the proposal, such as parking for the shopping center, stacking for the drive-through window, a second drive-through lane and safety with the potential number of cars entering and exiting the site. The trail connection is a positive feature of the proposal. On March 7, 2023, the City Council held a pre-application briefing for the proposed plans. The City Council highlighted some issues of concern similar to the Planning Commission, such as parking, traffic, and queuing in the drive-through facility. Additional comments centered around whether Taco Bell is right for this location and could the site be developed without a drive-through.

The applicant submitted a formal land use application on February 6, 2024. The Board of Architectural Review held a work session with the applicant to discuss the preliminary architecture and landscaping as proposed development is subject to a Major Certificate of Appropriateness. Staff provided written first submission comments to the applicant’s representative on April 5, 2024. Staff met with the applicant and their representative on April 26, 2024, for a Development Review Team meeting to discuss comment letter #1. A follow-up Development Review Team meeting was held on May 24, 2024, to discuss the applicant’s plan to address several items for the proposal.

On June 10, 2024, the Planning Commission held a post-submission work session to discuss a land use application to amend the Zoning and General Development Plan for Boulevard Marketplace Shopping Center in the CR Commercial Retail District and Architectural Control Overlay (ACOD); and a Special Use Permit to allow a 2,256 square foot restaurant with a drive-through facility to replace an approved 5,100 square foot commercial retail building on approximately 0.66 acres. Planning Commission provided comments and questions that covered several issues such as:

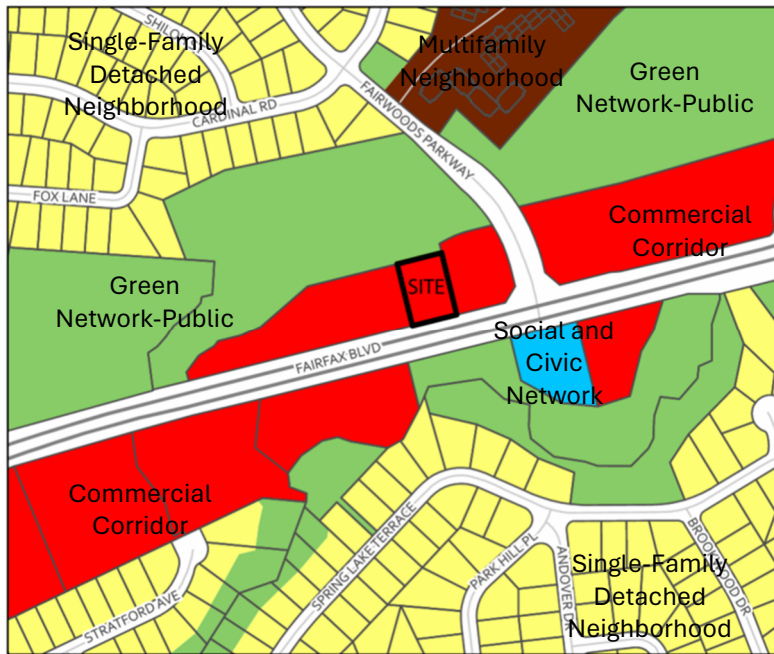
- The drive-through facility does not have a bypass lane
- The drive-through facility is separated from the parking area so a bypass lane may not be necessary.
- Trail connection is important and should be maintained. The trail should continue to Fairfax Boulevard.
- Clarification that the parking lot for Patient First is not included in the overall number of spaces for the shopping center
- Discussion on access through Patient First.
- The TIS shows a few intersections with problematic level of service and the nearby fire station creates additional challenges to the intersection of Fair Woods Parkway and Fairfax Boulevard.
- Discussion on the other restaurants that are open late at night.
- Sound may be an issue for residences within 500 feet of the site

### **COMPREHENSIVE PLAN**

Land Use: The Comprehensive Plan provides a general plan and communicates a vision for future land use and development in the city; while the Zoning Ordinance provides the regulatory mechanism to ensure the new development and changes in land use are consistent with the vision. Figure 1 (next page) illustrates the Future Land Use Map for the subject property and surrounding area:

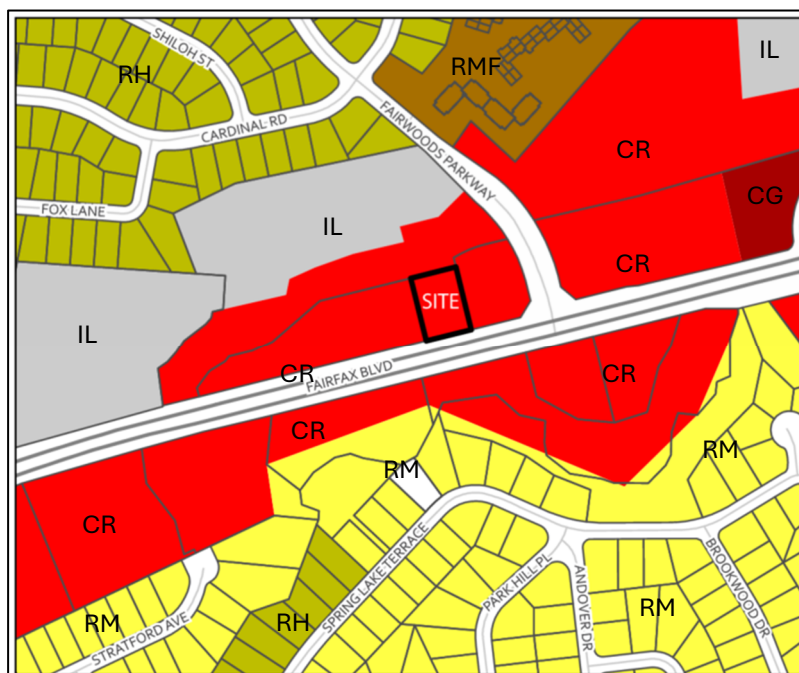


Figure 1: Comprehensive Plan



The Commercial Corridor Place Type includes a mix of retail, restaurant, service, medical, office, and other commercial uses. Limited manufacturing and other light industrial uses may also be considered. Heavy industrial uses should not be added or expanded beyond areas where they currently exist (such as the tank farm on Pickett Road). Residential uses are not recommended in Commercial Corridors. Commercial areas should accommodate access via a variety of transportation modes and be accessible to adjacent neighborhoods via pedestrian and bicycling facilities (City of Fairfax, 2035 Comprehensive Plan, Page 31). See Attachment 2 for Policy Guidance. Figure 2 (below) illustrates the zoning districts for the subject property and the surrounding properties.

Figure 2: Current Zoning



The surrounding zoning district to the east and west is CR Commercial Retail with uses that range from restaurants, medical facilities, and retail uses. To the north of the site is undeveloped land owned by the City of Fairfax and to the south of the site is a restaurant, city owned park land, offices, and a public facility (Fire Station 33).

## **Requests**

### **General Development Plan Amendment**

In 2008, a General Development Plan was approved for Boulevard Marketplace Shopping Center on 3.15 acres to allow a total of 17,300 square feet of commercial space including a 4,300 square foot bank building. A modification to the plan was approved to increase the amount of general retail space and decrease the area for the bank. In 2011, the bank parcel was subdivided from the shopping center establishing Parcel D (0.66-acres) and was sold to the current property owner (JDC Boulevard, LLC). In 2012, the Boulevard Marketplace Shopping Center was completed with the bank site being improved as pad site for a future tenant. In 2017, an amendment to the existing General Development Plan was approved to replace the approved 3,500 square foot bank building and drive-through with a 5,100 square foot commercial retail building. To date, the approved retail building has not been constructed and Parcel D remains a vacant pad site in the Boulevard Marketplace Shopping Center. The applicant is seeking to amend the existing Zoning and General Development Plan to allow for the development of Parcel D, which has an approved approximately 5,100 square foot retail building on a vacant pad site. Parcel A (Boulevard Marketplace Shopping Center) would remain unchanged. The existing building and supporting infrastructure improvements for the shopping center would not be impacted by the proposed redevelopment of Parcel A. The existing shopping center has approximately 112 parking spaces.

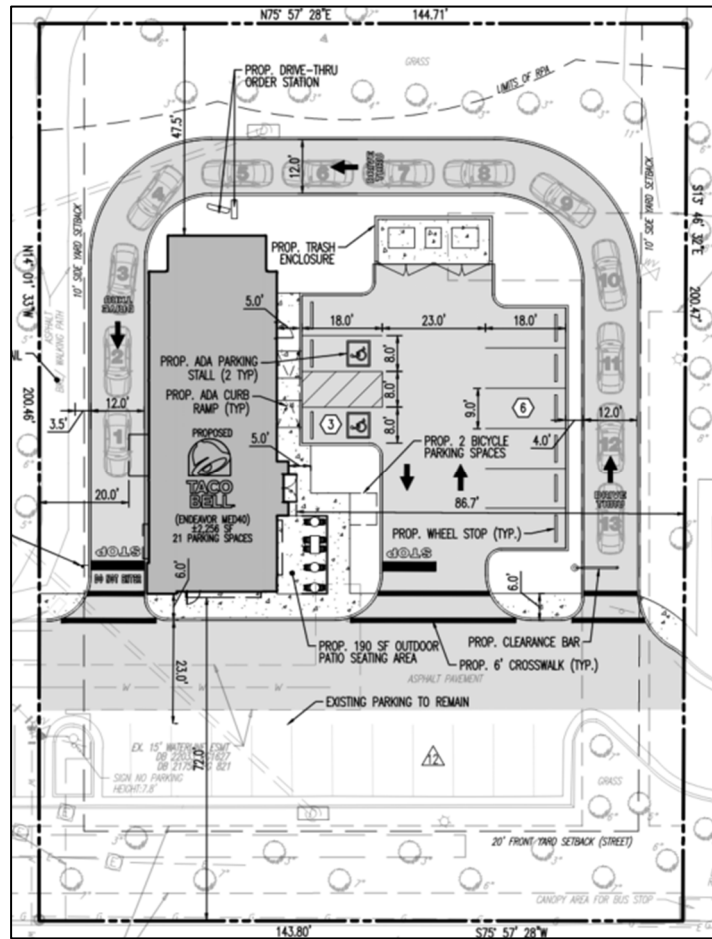
### **Description of the General Development Plan**

The applicant, ABTB Mid-Atlantic, LLC, is the contract lessee for a 0.66 +/- acre parcel in the Boulevard Marketplace Shopping Center. The applicant proposes to replace an approved 5,100 square foot commercial retail building with a 2,256 square foot restaurant with a drive-through. The general development shows a drive-through facility that wraps around a one-story 2,256 square foot building and parking lot area for customers. The applicant has provided nine (9) spaces adjacent to the entrance of the east side of the building and twelve (12) spaces that would be shared with other uses in the shopping center for a total of 21 spaces. The proposed drive-through facility has 13 stacking spaces from the drive-through window on the west side of the building and nine (9) spaces from the order station. The applicant has proposed a 190 square foot outdoor patio seating area near the main entrance to the restaurant. The applicant has provided pedestrian access interior to the site leading to the proposed building from the parking lot areas.

The primary access point to the proposed restaurant is through the existing internal connections to the shopping center with two access points along Fairfax Boulevard. At the western end of the shopping center there is a signalized intersection at Boulevard Market Place and Fairfax Boulevard with full turning movements onto Fairfax Boulevard, while the secondary access point to shopping center is a right-in/right-out onto Fairfax Boulevard. The shopping center has an east-west parallel street interior to the site that provides patrons access through the shopping center to an inter-parcel connection to Patient First at the intersection of Fairfax Boulevard and Fair Woods Parkway. On the west side of Parcel D is an existing shared use path that connects to the George T. Synder Trail to the north of the site and along the frontage of the shopping center is a 6-foot sidewalk. To the north of the building footprint and drive-through lane is an area that is in the floodplain and the Chesapeake Bay Resource Protection Area with existing plantings.

Figure 3 (below) shows the building footprint and limits of disturbance for the proposed general development plan.

Figure 3: General Development Plan

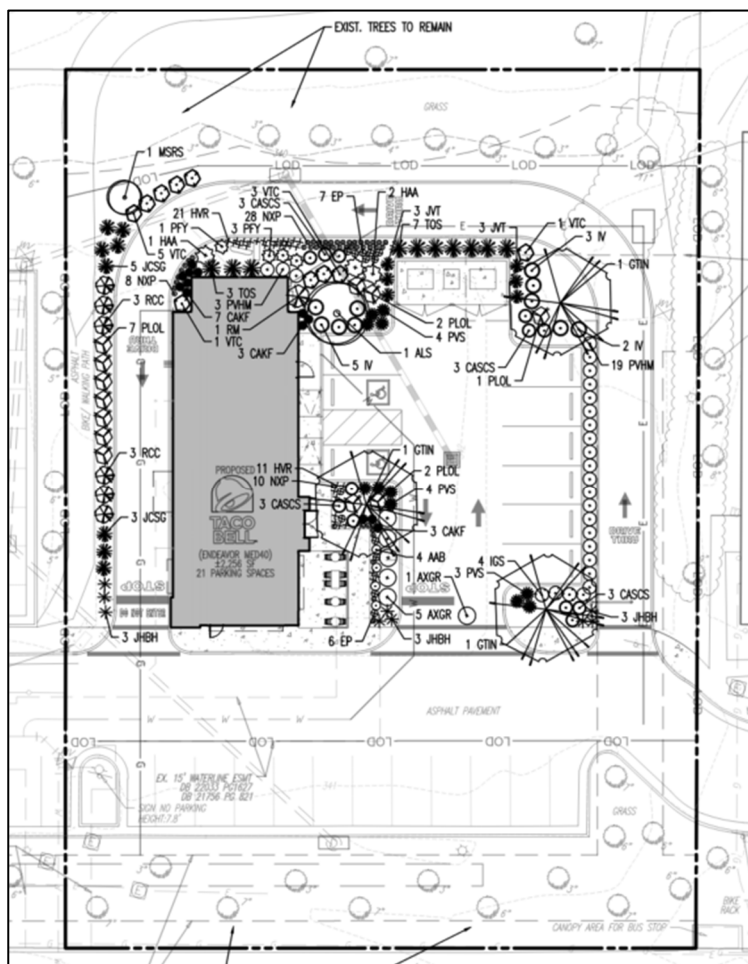


Building Coverage: Building coverage is the percentage of lot area that is permitted to be covered by buildings, including both principal structures and accessory buildings (Zoning Ordinance, Section 1.5.7.A.1, Page 1-5). The applicant has proposed a building coverage of 7.8%, which is below the maximum 60% in the CR Commercial Retail.

Lot coverage: Lot coverage is the percentage of lot area that may be covered by buildings, including both principal and accessory structures, impervious surfaces such as driveways, uncovered porches or patios, swimming pools, or roof overhangs of more than three feet (Zoning Ordinance, Section 1.5.7.B, Page 1-6). The applicant has proposed a lot coverage of 39%, which is below the maximum 85% in the CR Commercial Retail.

Tree Canopy: The landscape plan as submitted is incomplete. Figure 4 (next page) illustrates the proposed landscape plan for the general development plan.

Figure 4: Landscape Plan



The applicant states “that the existing street trees, landscaping, streetlights, and streetscape along Fairfax Boulevard will be retained. Ensuring that the established streetscape along this segment of Fairfax Boulevard will remain uniform and consistent” (Statement of Justification, Page 2).

Utilities: All on-site utilities shall be installed underground at the applicant’s expense in accordance with city and applicable utility company standards; provided that temporary overhead facilities required for construction purposes shall be permitted (Section 4.11.B). When the proposed development will result in moving or relocating existing overhead utilities located in adjoining rights-of-way, the applicant shall be responsible for placing such utilities underground and dedicating any additional right-of-way or easement that is necessary. Equipment such as electric distribution transformers, switch gear, meter pedestals and telephone pedestals which is normally installed above ground in accordance with generally accepted utility practice for underground distribution may be so installed (Section 4.11.C). The shopping center has underground utilities to serve the existing buildings. The applicant is proposing to relocate an existing transformer box to make way for the drive-through facility.

Transportation: The proposed development is bound by Fairfax Boulevard, Boulevard Market Place, and Fair Woods Parkway. Fairfax Boulevard is classified as a principal arterial street with a posted speed limit of 35 MPH near the site and this segment of Fairfax Boulevard is three lanes in each direction. Fair Woods Parkway is a two-lane local road with on-street parking and a posted speed limit of 25 MPH. Table 3 (next page) provides a summary of proposed trips:

Table 3: Trip Generation

Land Use (ITE Code)	Size (Units)	AM Peak Hour			PM Peak Hour			Weekday Daily		
		In	Out	Total	In	Out	Total	In	Out	Daily
Fast Food Restaurant with Drive-Through Window (934)	2,256 (Sq. Ft.)	52	49	101	39	36	75	528	527	1,055

**Public Transportation Facilities:** The subject property has several mass transportation routes in the vicinity of the subject property, such as MetroBus, and the City’s CUE Bus Gold and Green Routes.

**Circulation/Pedestrian Access:** The Multi-Modal Transportation Plan recommends a 10-foot sidewalk on Fairfax Boulevard. The applicant has proposed a 6-foot sidewalk between the building and the parking lot. The site has an existing trail on the western property line that connects to the George T. Snyder Trail.

**Parking:** The proposed use (restaurants or food service) is subject to 1 space per 200 square feet of floor area. Table 4 (below) summarizes the required parking requirements.

Table 4: Parking Requirements

Use	Units	Zoning Requirement	Required Spaces	Proposed Spaces
Restaurants or food service	2,256 sf	1 space per 200 sq. ft.	11	21

The applicant is required to provide 11 parking spaces and proposes 21 parking spaces in the general development plan.

**Loading Zone:** The applicant proposes a 2,256 square foot commercial building. Due to the size of the proposed building, the applicant is not required to provide a loading space (Section 4.2.9 – Off-Street Loading Requirements).

**Bicycle Parking:** Section 4.2.8.C of the Zoning Ordinance states that bicycle parking shall be in a visible, well-illuminated area that does not conflict with automobile or pedestrian traffic. The requirement is 2 bicycle spaces based on the number of required parking spaces (between 1-40).

**Stormwater Management:** Stormwater BMPs, on-site detention facilities, and on-site drainage facilities shall be designed and maintained in such a manner as to minimize economic and environmental costs to the city and its inhabitants (Zoning Ordinance, Section 4.16.2, Page 4-77). Stormwater management facilities for the site were provided in the original construction of the shopping center.

**Special Use Permit**

- **Special Use Permit** for drive-through windows/facilities in the CR Commercial Retail

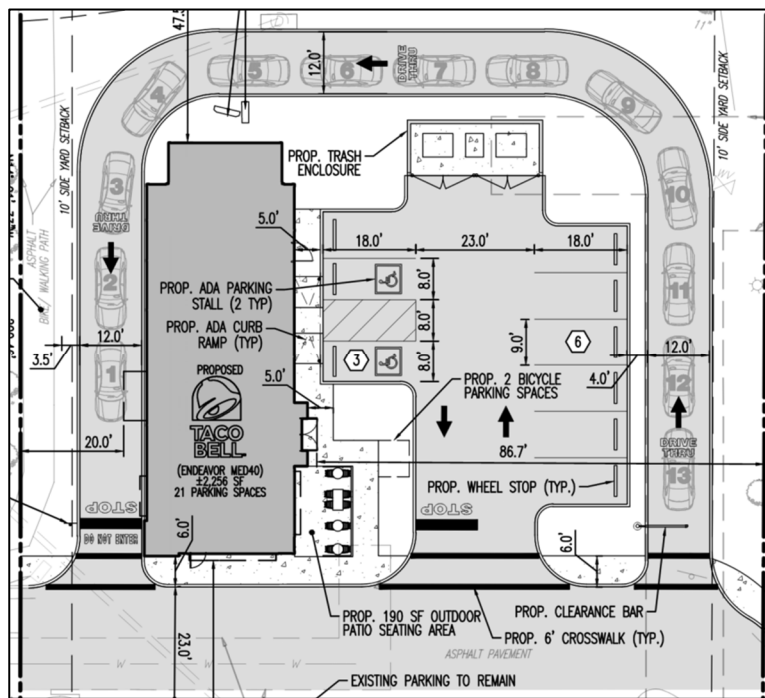
The Zoning Ordinance defines a drive-through facility as “a window or other facility designed to enable a person to transact business while remaining in a motor vehicle.” The development standards for drive-through windows/facilities (Section 3.5.5.D.10 of the Zoning Ordinance) are outlined below:

10. Drive-through windows/facilities

- (a) No drive-through windows/facilities shall be permitted on the side of a building adjacent to or facing any residential district;
- (b) Stacking spaces shall be provided in accordance with the requirements of Section 4.2;
  - (1) The location of each drive-through stacking spaces and the direction of flow shall be clearly demarcated with pavement marking;
  - (2) Where a stacking lane abuts a parking aisle, the area required for the stacking lane shall be in addition to that required for the aisle;
  - (3) No drive-through stacking lane shall be located between proposed parking spaces and any public entrance to such establishment; and
- (c) Screening shall be provided in accordance with the requirements of Section 4.5.

The applicant states that “the drive-through window will be located on the side of the proposed building that is adjacent to the shopping center and does not face any residential districts. The applicant proposes 13 stacking spaces, which exceeds the 10 spaces required, where the location and the direction of the flow of these spaces will be clearly demarcated with pavement markings. In addition, the stacking lane does not abut a parking aisle nor is it located between proposed parking spaces or the public entrance to the restaurant.” (Statement of Justification, Page 3). Figure 5 (below) illustrates the number of cars stacked in the drive-through facility.

Figure 5: Drive-Through Facility/Window



In addition to Section 3.5.5.D.10, the applicant shall demonstrate compliance with stacking space requirements for drive-through windows/facilities (Section 4.2.7 of the Zoning Ordinance). Table 5 (next page) provides the minimum number of stacking spaces:



Table 5: Stacking Space Requirements

Facility or Use	Minimum Spaces	Measures From
Drive-through windows/facilities	6	Order Box
Drive-through windows/facilities	4	Service window, or from order box to service window

The proposed drive-through facility has 13 stacking spaces from the drive-through window on the west side of the building and nine (9) spaces from the order station. The applicant is showing stop controls into the interior parking lot and at the exit for the drive-through.

Code Administration: The drive lane down the center of the parking lot will need to be designated a Fire Lane and the applicant will need to provide fire apparatus turning movements (both directions) into the fire lane using Tower Ladder 403 specifications. Also, based on the drive lane in the parking area will be a fire lane, and the service road in front of the property is already (and will remain) a fire lane, the applicant needs to share where delivery trucks will park.

City Schools: No impact on City Schools as the proposal is for non-residential use.

Fiscal Impact: City staff is preparing the fiscal impact analysis.

Historic Resources: No issues with the proposed land use application.

Parks and Recreation: No impact on the City Parks as the proposal is for non-residential use.

Police: City Police provided the applicant with comments based on the principles of Crime Prevention Through Environmental Design (CPTED).

Proposed Changes to First Submission GDP Plans

The applicant has proposed to make changes to the plans to reflect the comments provided by staff upon review of the first submission. These changes include additional information regarding seating capacity, operational hours, setbacks including Resource Protection Area boundary, improvements to pedestrian connections, a fire marshal plan, and an erosion and sediment control plan. See Attachments 9-11 for the applicant’s response to staff comments, the updated General Development Plan Amendment/SUP Plan, and revised Traffic Impact Study. These documents have not been formally filed through a resubmission to City staff as additional revisions may be considered following the work sessions with the Planning Commission and City Council.

Attachments

- A1 – Statement of Justification
- A2 – Policy Guidance
- A3 – Certified Plat
- A4 – General Development Plan
- A5 – Traffic Impact Study
- A6 – Certificate of Appropriateness Work Session Staff Report
- A7 – Statement of Design Intent
- A8 – Certificate of Appropriateness Package
- A9 – Revised General Development Plan (not formally submitted)
- A10 – Revised Traffic Impact Study (not formally submitted)
- A11 – 1<sup>st</sup> Submission Comment Response Letter

**PREPARED BY:**



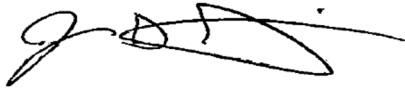
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Albert Frederick  
Senior Planner

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Jason D. Sutphin  
Community Development Division Chief

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Brooke Hardin  
Director, Community Development & Planning

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**ABTB Mid-Atlantic, LLC**

**STATEMENT OF JUSTIFICATION**

**January 18, 2024**

Please accept the following as a Statement of Justification in support of the submitted General Development Plan Amendment and Special Use Permit applications to allow the development of 10120 Fairfax Boulevard with an approximately 2,256 square foot restaurant with a drive-through window/facility. This Statement of Justification is submitted in conjunction with the General Development Plan Amendment/Special Use Permit Plat, prepared by Dynamic Engineering Consultants, P.C. dated January 18, 2024, (the “GDPA/SUP Plat”), and other submitted supporting materials. The contents of this Statement of Justification address the approval considerations for the submitted applications, as set forth in Sections 6.4.9 and 6.7.7 of the City of Fairfax Zoning Ordinance (the “Zoning Ordinance”).

ABTB Mid-Atlantic, LLC (the “Applicant”) is the contract lessee of an approximately 0.66 acre property located in the City of Fairfax. The property consists of one (1) tax parcel identified among the City of Fairfax’s tax assessment records as 47-4-23-000-D (the “Subject Property”). The Subject Property is currently a vacant pad site that is part of the Boulevard Marketplace Shopping Center (the “Shopping Center”). Zoned to the CR (Commercial Retail) District, the Subject Property is included in a Generalized Development Plan (“GDP”) that was approved by City Council in 2008, subject to proffers dated June 2, 2008, to allow a total of 17,300 square feet of commercial space, including a 4,300 square foot bank building with drive-through on the Subject Property. In 2011, the Shopping Center was subdivided into two separate parcels, including the Subject Property and the adjacent parcel identified as 47-4-23-000-A (“Parcel A”). In 2017, City Council approved a GDP amendment, which converted the previously approved drive-through bank on the Subject Property to an approximately 5,100 square foot retail building. To date, the approved retail building has not been constructed, and the Subject Property remains vacant.

**PROPOSED DEVELOPMENT**

The Applicant proposes to redevelop the Subject Property with a drive-through Taco Bell restaurant. No modifications or improvements are proposed to Parcel A, which will remain in its existing condition. The proposed development will provide a high-quality, active commercial use that will revitalize an underutilized parcel. As shown on the submitted GDPA/SUP Plat, the Applicant proposes a commercial use that will be consistent with the recommendations of the Comprehensive Plan and will be compatible with the commercial character of the surrounding area. As illustrated on Sheet 3 of the GDPA/SUP Plat, the proposed restaurant with drive-through will consist of an approximately 2,256 square-foot, one story building. The proposed drive-through lane will wrap around the new building and parking area and will accommodate 13 stacking spaces, which exceeds the minimum number required by the Zoning Ordinance. A total of 21 parking spaces are provided on the Subject Property, including 12 spaces along the existing drive aisle. Bicycle parking and a small outdoor dining patio area will also be provided proximate to the restaurant’s main entrance.

The Subject Property is particularly well-suited for the proposed use given its location along the City's main commercial corridor. In addition, the proposed use is consistent with the existing pattern of development along the Fairfax Boulevard corridor, which includes a number of existing drive-through restaurant uses. Primary access to the Subject Property will be through the existing internal circulation of the Shopping Center. The Shopping Center is served by two existing vehicular access points along Fairfax Boulevard – including one signalized full-movement access and one right-in/right-out access. An additional right-in/right-out vehicular access is provided to the adjacent Patient First parcel, which provides for inter-parcel vehicular access to the Subject Property. No changes are proposed to the existing access or on-site circulation conditions associated with the Shopping Center. To accommodate adequate pedestrian circulation, the Applicant will incorporate a 6-foot wide sidewalk between the proposed building and parking areas. In addition, the Applicant proposes to maintain an existing 8-foot wide trail along the western property line that connects to the George Snyder Trail to the north of the site. A 3.5-wide landscape strip is proposed between the drive-through lane and the 8-foot wide trail as shown on the GDPA/SUP Plat to provide an additional buffer between pedestrians and vehicles in the drive-through lane. Finally, while the northern portion of the Subject Property includes Resource Protection Area (“RPA”) and floodplain, none of the Applicant's proposed site improvements will encroach into these environmentally sensitive areas. All site work is limited to previously disturbed areas of the site. No changes are proposed to the proffers associated with the prior approvals.

The proposed building is characterized by attractive architecture, the provision of quality landscaping, and a reduction of impervious surface. The proposed building will be architecturally compatible with the adjacent buildings in the Shopping Center through the use of compatible masonry and cornice details that are consistent with the materials of the two existing buildings on Parcel A. Building materials may include, but are not limited to brick, Hardie cement board panels, E.I.F.S, and other quality materials. The proposed restaurant building will be one story at a maximum height of twenty-five (25) feet. Quality landscaping is proposed, including robust vegetation around portions of the drive-through lane as shown on the GDPA/SUP Plat. This will result in a significant improvement over the existing landscape conditions on the Subject Property today. The existing street trees, landscaping, streetlights and streetscape along Fairfax boulevard will be retained, ensuring that the established streetscape along this segment of Fairfax Boulevard will remain uniform and consistent. In the end, the proposed development will result in increased green open space and an overall decrease of impervious surface.

## GDP AMENDMENT APPLICATION

To permit the development of a restaurant use on the Subject Property, the Applicant requests an amendment to the previously approved rezoning and general development plan, which currently depicts an approximately 5,100 square foot retail building on this vacant pad site. No changes are proposed to the previous proffered commitments, all of which have been fulfilled. This proposed amendment fulfills each of the approved considerations set forth in Section 6.4.9 of the Zoning Ordinance:

The proposed use is in substantial conformance with the Comprehensive Plan. The Future Land Use Map for the City of Fairfax indicates that the Subject Property is planned for a

Commercial Corridor Place Type, which supports commercial uses, including restaurants with drive-through facilities. In addition, the surrounding land use designations are similarly Commercial Corridor. In redeveloping the existing, underutilized parcel into a more active, high-quality community-serving use, the proposed redevelopment is consistent with the stated objectives of the Comprehensive Plan and will provide a benefit to the City by promoting the economic viability of the Fairfax Boulevard corridor. In addition, the proposed amendment to convert the approved retail building to a restaurant with a drive-through remains highly suitable and consistent with the current CR District. Currently, the Subject Property is sufficiently served by public transportation facilities, and the existing utility infrastructure is adequate for the proposed uses. Further, the proposed development is compatible with the surrounding area, as the Subject Property is entirely surrounded by other commercial uses, including an adjacent shopping center with various restaurants, a salon, and convenience stores as well as a medical office. Finally, the proposed development is in keeping with the stated purpose of the CR District. Accordingly, the proposed development will further enhance the growth of commercial activities planned for the Fairfax Boulevard corridor by adding a community-serving use that is convenient for the City's residents.

#### SPECIAL USE PERMIT

In conjunction with the proposed amendment to the rezoning and general development plan, the Applicant requests approval of a special use permit application pursuant to Section 3.5.5.D.10 of the Zoning Ordinance to allow a drive-through window/facility in connection with the proposed restaurant use. As shown on the GDPA/SUP Plat, the drive-through window will be located on the side of the proposed building that is adjacent the Shopping Center and does not face any residential districts. The Applicant proposes 13 stacking spaces, which exceeds the 10 spaces required, where the location and the direction of the flow of these spaces will be clearly demarcated with pavement markings. In addition, the stacking lane does not abut a parking aisle nor is it located between proposed parking spaces or the public entrance to the restaurant.

In addition, the proposed drive-through window/facility fulfills the approval considerations for a special use permit set forth in Section 6.7.7 of the Zoning Ordinance. The integration of a drive-through window with the proposed restaurant use is consistent with the Comprehensive Plan as a community-serving, commercial use that is supported by the Commercial Corridor land use designation. The use is compliant with all applicable Zoning requirements, related to setbacks, buffers, lighting, signage, parking, and other related requirements. In addition, the inclusion of the drive-through window/facility will not adversely affect the health or safety of persons residing or working in the neighborhood of the proposed use nor will it have a negative, adverse effect on public welfare, property, and improvements in the neighborhood. As stated above, the surrounding area is entirely commercial in nature and includes other existing restaurants. Thus, the addition of a new restaurant with a drive-through window/facility on the Subject Property will have a positive, beneficial impact on the surrounding neighborhood that is in keeping with the area, as it offers a new and convenient dining option for the City's residents, workers, and visitors.



## CONCLUSION

The Applicant's proposal presents an opportunity to develop and activate a vacant pad site with a vibrant, high-quality community-serving use that advances the Comprehensive Plan's objectives of enhancing commercial activities along the Fairfax Boulevard commercial corridor. The proposed development will generate increased economic activity and contribute to the City's continued fiscal growth. The Applicant is eager to bring the first Taco Bell to the City and is committed to ensuring that the proposed development fits into the fabric of Fairfax.

## Policy Guidance

### 2035 Comprehensive Plan Policies

#### Commercial Corridor Place Type

The Commercial Corridor Place Type, identified in red on the Future Land Use Map, includes a mix of retail, restaurant, service, medical, office, and other commercial uses. Limited manufacturing and other light industrial uses may also be considered. Heavy industrial uses should not be added or expanded beyond areas where they currently exist (such as the tank farm on Pickett Road). Residential uses are not recommended in Commercial Corridors. Commercial areas should accommodate access via a variety of transportation modes and be accessible to adjacent neighborhoods via pedestrian and bicycling facilities.

Commercial Corridor Place Types can accommodate a variety of buildings from small footprint retail buildings to multi-story office buildings. The desired orientation and placement of buildings on a Commercial Corridor site is primarily dependent on the adjacent Street Type. For sites located along Commercial Mains, buildings should have similar setbacks and building orientation as recommended for the nearby Activity Centers. Parking is encouraged in above-ground structures or underground, should be provided to the side or rear of buildings, and should be screened from view from the right-of-way by building mass or landscaping. For sites located along Boulevards or other street types, buildings should be located near front property lines with parking provided to the side or rear. Direct pedestrian access should be provided from the sidewalk in the right-of-way to primary building entrances. Predicated on the underlying zoning district, the Commercial Corridor Place Type supports a density of a minimum Floor Area Ratio (FAR) of 0.4 for commercial development and a maximum building height of 3 stories/35 feet to 5 stories/60 feet. Refer to the City of Fairfax Design Guidelines for more specific guidance on site design.

Neighborhoods: Neighborhood – the places where we live, learn, play, and increasingly work – constitute the largest geographical use of land in the city, though physical boundaries are not the only thing defining character trait of a neighborhood. The City’s neighborhoods each have their own unique character and offer a variety of housing and lifestyle opportunities. Neighborhoods are supported by a separate Guiding Principle in this Plan due to their importance to residents. City growth and development policies must both preserve the quality of neighborhoods and protect neighborhoods from adverse consequences of growth. However, this should not imply that Fairfax’s neighborhoods should remain static. Well-designed and properly scaled infill can be an appropriate strategy to foster walkability, better amenities, and housing affordability (City of Fairfax 2035 Comprehensive Plan, Chapter 2: Land Use, Page 45).

#### Neighborhood Goal 1

Enhance neighborhood character.

Outcome N1.1: Infill housing that complements the character of surrounding homes in existing neighborhoods.

Action N1.1.1: Maintain regulatory standards to ensure infill housing fits in with the surrounding neighborhood context.

Neighborhood Goal 2

Provide neighborhood pedestrian connections.

Outcome 2.1: Residents of all abilities safely and easily move about the community.

Action 2.1.1: Identify opportunities for future open space and trails in neighborhoods that are currently deficient in offering these amenities.

Action 2.1.2: Expand existing pedestrian network to increase connectivity within neighborhoods and to other destinations.

Commercial Corridors and Activity Centers: Fairfax’s success in achieving the community’s vision for future development hinges upon effective growth strategies for the City’s areas of highest redevelopment potential. These areas will accommodate the majority of new commercial activity, higher density residential neighborhoods, and transportation improvements. Success in achieving this vision will be measured not by the magnitude of new investment, but rather by the attributes that can transform a disjointed pattern of development into an attractive and welcoming neighborhood. If the City’s Commercial Corridors and Activity Centers can be transformed into areas with attractive physical characteristics and a mix of uses, then the city will realize a major aspect of its goal to be a vibrant 21st century community. While higher intensity mixed-use redevelopment of older commercial properties can provide economic and social benefits to the community, these benefits would be most realized if concentrated in key areas to allow new developments to complement each other, avoid oversaturating the market, and minimize impacts to existing neighborhoods. These types of uses are primarily envisioned in Activity Centers, as indicated on the Future Land Use Map. While a mix of uses and connected street grids are envisioned in all Activity Centers, such development is always encouraged in the Old Town Fairfax and Northfax Activity Centers (City of Fairfax 2035 Comprehensive Plan, Chapter 2: Land Use, Page 49).

Commercial Corridors & Activity Centers Goal 1

Enhance Commercial Corridors.

Outcome CCAC1.1: Commercial Corridors with attractive physical characteristics that provide shopping, dining, services, and other businesses.

Action CCAC1.1.1 Encourage commercial redevelopment that offers amenities and atmosphere to attract top-tier commercial tenants.

Action CCAC1.1.2 Identify underutilized properties (i.e., buildings assessed at considerably less than the total property value) and, working with the City’s Economic Development Authority, encourage redevelopment.

Action CCAC1.1.3 Encourage creativity and architectural excellence in new commercial developments.

Action CCAC1.1.4 Develop urban design concept diagrams for small block and multi-block areas along the City's Commercial Corridors outside the Activity Centers.

Action CCAC1.1.5 Encourage tree-lined and heavily-landscaped property edges, particularly where surface parking is adjacent to the public rights-of-way.

Action CCAC1.1.6 Provide pedestrian and bicycle connections to nearby neighborhoods.

Outcome CCAC1.2: Tenants representing diverse business sectors that meet current and emerging trends in neighborhood-serving retail, service, and other business demands.

Action CCAC1.2.1 Strengthen existing retail businesses and expand choices to capture retail spending by residents.

Action CCAC1.2.2 Create a marketing plan to generate excitement about the current retail and service offerings.

Community Design: An attractive, well-designed City instills civic pride; improves the visual character of the community; creates a strong, positive image; and attracts quality developments. Community Design relates not just to what buildings look like, but to the spaces between buildings, as well as to the street and public realm. While accommodating new growth and change, consideration must be given to preserving significant elements of the community that contribute to the City's unique character. The intent of the Community Design and Historic Preservation Guiding Principle is to capitalize on unique features of the city in a manner reflecting the community's values and its connection to the history and traditions that distinguish it from other communities in the region (City of Fairfax 2035 Comprehensive Plan, Chapter 2: Land Use, Page 61).

Community Design and Historic Preservation Goal 1

Require high-quality, sustainable design.

Outcome CDHP1.2: Attractive buildings, inviting public spaces, and welcoming gateways that contribute to our economic vitality and unique character.

Multimodal Transportation: Transportation is about more than mere movement – transportation grants us access to the needs of everyday life. Sustainable, connected, and integrated transportation is fundamental to the success and livability of the city. Multimodal refers to the multiple ways people use to get around – car, bus, train, bike, walking, etc. – and a multimodal plan incorporates these various transportation modes into an efficient and connected system (City of Fairfax 2035 Comprehensive Plan, Chapter 2: Land Use, Page 67).

Multimodal Transportation Goal 2

Provide viable and attractive mobility choices.

Outcome MM2.1: Pedestrian safety is improved.

Action MM2.1.1: Fill critical gaps in the pedestrian network. Develop and act on a prioritized list of sidewalk improvements in the commercial areas and provide sidewalks on at least one side of every residential street in neighborhoods that are in agreement.

Action MM2.1.2: Ensure the pedestrian network is accessible to all and meets the requirements of the Americans with Disabilities Act (ADA).

Outcome MM2.2: The City's existing trail system, including the "Green Ribbon" parks and trail network, is connected and expanded.

Action MM2.2.1 Identify and fill gaps in the trail network. Find opportunities for future trails, complete connections to existing segments, implement projects proposed by the Parks and Recreation Master Plan, and pursue new trail connections to create a more functional trail network.

Action MM2.2.2 Connect the George Snyder Trail to the planned I-66 trail facility.

### Multimodal Transportation Goal 3

Integrate transportation with land use.

Outcome MM3.2: Walkability to and within Activity Centers and between neighborhoods is increased.

Action MM3.2.1 Whenever possible, increase connections – particularly nonmotorized connections – between neighborhoods, community facilities, and Activity Centers.

Action MM3.2.2 With development projects, break up large blocks to a more walkable scale. Pursue additional secondary and tertiary street network opportunities. Streets should be well-designed as complete streets and align at regular intersections for a continuous street grid.

Action MM3.2.4 Improve the overall pedestrian environment, including pedestrian crossings, street trees, and furnishing zones; buffering sidewalk from vehicle travel lanes; improved pedestrian scale lighting; and active ground floor uses along primary street edges.

Economic Vitality: Commercial properties tend to contribute more to the tax base than they consume in public expenditures because of relatively high real estate values and lower dependence on public services. The city has historically benefited from having a high ratio of commercial to residential development, resulting in a lesser tax burden on residences. As commercial properties have aged and new commercial development has slowed, this ratio has begun to shift. The City can offset this shift by supporting measures to increase values of existing commercial properties and encouraging new commercial development so that the value of nonresidential properties continues to comprise a greater proportion of the City's taxable properties (City of Fairfax 2035 Comprehensive Plan, Chapter 5: Economic Vitality, Page 117).

Economic Vitality Goal 1

Increase the City's ratio of commercial to residential real estate.

Outcome EV1.1: New development and redevelopment that maximize revenue generation from nonresidential buildings and uses.

Action EV1.1.1 Attract new commercial businesses while supporting and retaining existing businesses.

Economic Vitality Goal 2

Support diversification of the retail, service, and office sectors.

Outcome EV2.1: The retail and service sectors more effectively compete with other regional commercial sectors, resulting in increased desirability as a destination.

Action EV2.1.1 Attract new retail and service businesses representing sectors that have the ability to become regional destinations.



**GENERAL NOTES:**

- THE RECORD OWNER OF PARCEL D IS JDC BOULEVARD, LLC C/O ROSENTHAL PROPERTIES BY DEED DATED JULY 13, 2011 AND RECORDED AT THE FAIRFAX COUNTY REGISTRY OF DEEDS IN BOOK 21756, PAGE 821, AND IS SHOWN AS TAX MAP # 47-4-23-000D. THE RECORD OWNER OF PARCEL A IS SCC BOULEVARD MARKETPLACE LLC C/O SITE CENTERS CORP BY DEED DATED JULY 13, 2011 AND RECORDED AT THE FAIRFAX COUNTY REGISTER OF DEEDS IN BOOK 21756, PAGE 821, AND IS SHOWN AS TAX MAP # 47-4-23-000A.
- SURVEY WAS PREPARED WITHOUT BENEFIT OF A CERTIFIED TITLE REPORT AND MAY NOT SHOW ALL DEDICATIONS, EASEMENTS, RESTRICTIONS OR COVENANTS EXISTING IN THE CHAIN OF TITLE.
- THIS SURVEY IS BASED ON THE FIELD CONDITIONS AS OF NOVEMBER 2022, JANUARY 2024, AND INFORMATION FROM DEEDS AND PLATS.
- TOTAL AREA OF PARCEL A IS APPROXIMATELY 2.49 ACRES AND PARCEL D IS APPROXIMATELY 0.66 ACRES.
- BEARINGS SHOWN HEREON ARE IN REFERENCE TO GRID NORTH, VIRGINIA NORTH STATE PLANE COORDINATE SYSTEM UNLESS OTHERWISE NOTED, NAD83. ELEVATIONS DEPICTED HEREON ARE IN REFERENCE TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88), BASED GPS OBSERVATIONS.
- UTILITY INFORMATION DEPICTED HEREON IS COMPILED USING PHYSICAL SURFACE EVIDENCE LOCATED IN THE FIELD IN CONJUNCTION WITH ANY RECORD INFORMATION AVAILABLE AT THE TIME OF THIS SURVEY, AND MAY NOT NECESSARILY REPRESENT ALL EXISTING UTILITIES. CONTRACTORS AND/OR DESIGNERS NEED TO CONTACT 811 AND FIELD VERIFY EXISTING UTILITIES PRIOR TO ANY CONSTRUCTION AND/OR EXCAVATION TICKET # A229900342-00A.
- A WETLAND DELINEATION WAS NOT PERFORMED AS PART OF THIS SURVEY, AND NO WETLAND FLAGS WERE OBSERVED OR PROVIDED TO THIS SURVEYOR.
- STREAM AND WETLAND INFORMATION WAS GATHERED FROM GIS FROM THE CITY OF FAIRFAX META DATA UPDATED ON AUGUST 25, 2022. NO FIELDWORK WAS PERFORMED TO GATHER THIS INFORMATION.
- NO EVIDENCE OF RECENT EARTHWORK WAS OBSERVED DURING THE COURSE OF THIS SURVEY.
- NO POSSIBLE ENCROACHMENTS OBSERVED.
- REFERENCE IS MADE TO THE FOLLOWING PLANS:
  - CONSOLIDATION AND RE-SUBDIVISION OF PARCEL 2-B-1 EATON TRACT DEED BOOK 9311 PAGE 115 AND PORTION OF PARCEL 4.
- ALL PARKING SPACES SHOWN HEREON ARE OPEN.
  - NUMBER OF REGULAR PARKING SPACES = 17
  - NUMBER OF HANDICAP PARKING SPACES = 2

**SURVEYORS CERTIFICATE**

I HEREBY CERTIFY TO THE BEST OF MY PROFESSIONAL KNOWLEDGE, BELIEF AND INFORMATION THAT THIS "EXISTING CONDITIONS DRAWING" HAS BEEN PREPARED IN ACCORDANCE WITH ADMINISTRATIVE CODE FOR LAND SURVEYS IN VIRGINIA

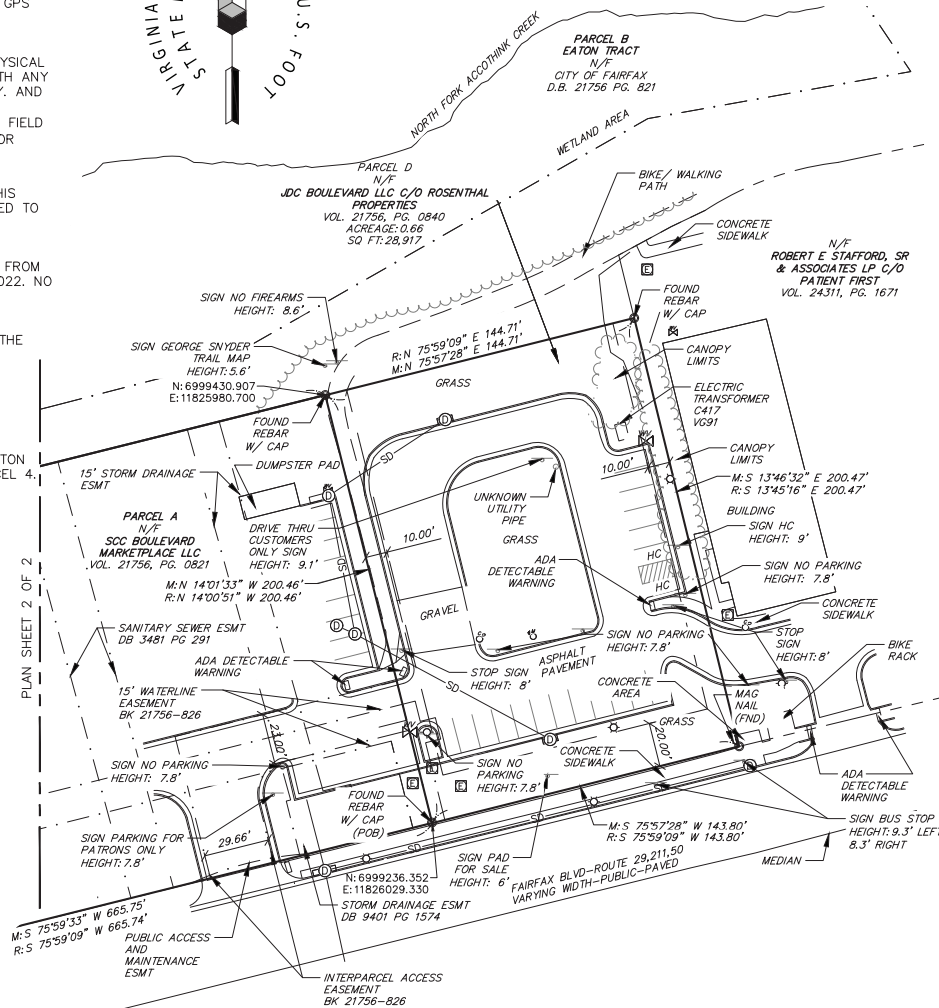
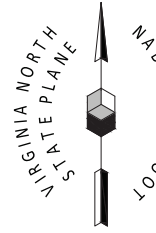
**Michael Gress**

Digitally signed by Michael Gress  
DN: cn=Michael Gress, email=mgress@cornerstoneenergy.com, c=US

MICHAEL GRESS; VAPLS 0403003638

DATE

CORNERSTONE ENERGY SERVICES INC.  
111 CHINQUAPIN ROUND ROAD  
SUITE 203, ANNAPOLIS, MD 21401



**LEGEND**

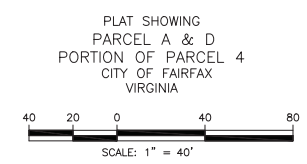
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- - -	ABUTTER LINE
---	SETBACK
---	EASEMENT
---	FLOODPLAIN
○	IRON PIPE/ROD FOUND
●	REBAR SET
---	BUILDING
---	EDGE PAVEMENT
---	EDGE CONCRETE
---	PAVEMENT PAINT
---	EDGE GRAVEL
---	CURB LINE
---	TREELINE
○	BOLLARD/POST
W	SIGN
W	WATER
W	WATER GATE VALVE
W	WATER SHUT OFF
W	WATER METER
W	HYDRANT
W	SEWER CLEANOUT
W	STORM DRAIN
W	DRAINAGE MANHOLE
W	ELECTRIC BOX
W	LIGHT POLE

APPROVED FOR  
CITY OF FAIRFAX, VIRGINIA

ZONING OFFICIAL

DATE

APPROVAL VOID IF PLAT IS NOT OFFERED FOR RECORD WITHIN 180 DAYS AFTER DATE THEREOF



REV:	BY:	DATE:	STATUS:
THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION. ANY ALTERATIONS, AUTHORIZED OR OTHERWISE, SHALL BE AT THE USER'S SOLE RISK.			

**Cornerstone**  
Energy Services

111 CHINQUAPIN ROUND ROAD  
SUITE 203  
ANNAPOLIS, MD 21401

410-220-5615

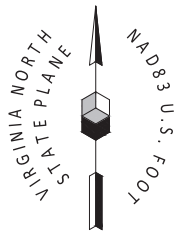
EXISTING CONDITIONS SURVEY

OF: 10120 FAIRFAX BLVD  
FAIRFAX, VA

FOR: SOUTH PAW; DAVID PANELLA  
354 AMITY ROAD, SUITE 200  
WOODBRIIDGE, CT 06525

DRAWN	CHECKED
AR/MA	MJG
PROJECT NO.	DATE
22320.10	1/30/24
SHEET SIZE	SCALE
18"X24"	1"=40'

SHEET 1 OF 2



**SURVEYORS CERTIFICATE**

I HEREBY CERTIFY TO THE BEST OF MY PROFESSIONAL KNOWLEDGE, BELIEF AND INFORMATION THAT THIS "EXISTING CONDITIONS DRAWING" HAS BEEN PREPARED IN ACCORDANCE WITH ADMINISTRATIVE CODE FOR LAND SURVEYS IN VIRGINIA

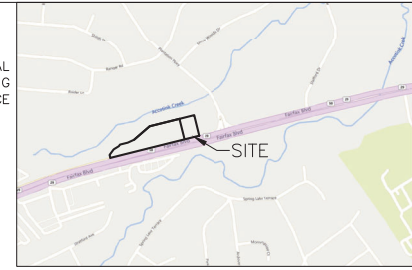
**Michael Gress**

Digitally signed by Michael Gress  
DN: cn=Michael Gress, email=mgress@cornerstoneenergy.com, c=US  
Date: 2024.02.27 14:58:56-0500

MICHAEL GRESS; VAPLS 0403003638

DATE

CORNERSTONE ENERGY SERVICES INC.  
111 CHINQUAPIN ROUND ROAD  
SUITE 203, ANNAPOLIS, MD 21401



**LEGEND**

- PROPERTY LINE
- ABUTTER LINE
- SETBACK
- EASEMENT
- FLOODPLAIN
- IRON PIPE/ROD FOUND
- REBAR SET
- BUILDING
- EDGE PAVEMENT
- EDGE CONCRETE
- EDGE GRAVEL
- CURB LINE
- TREELINE
- BOLLARD/POST
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- WATER
- WATER GATE VALVE
- WATER SHUT OFF
- WATER METER
- HYDRANT
- SEWER CLEANOUT
- STORM DRAIN
- DRAINAGE MANHOLE
- ELECTRIC BOX
- LIGHT POLE

APPROVED FOR  
CITY OF FAIRFAX, VIRGINIA

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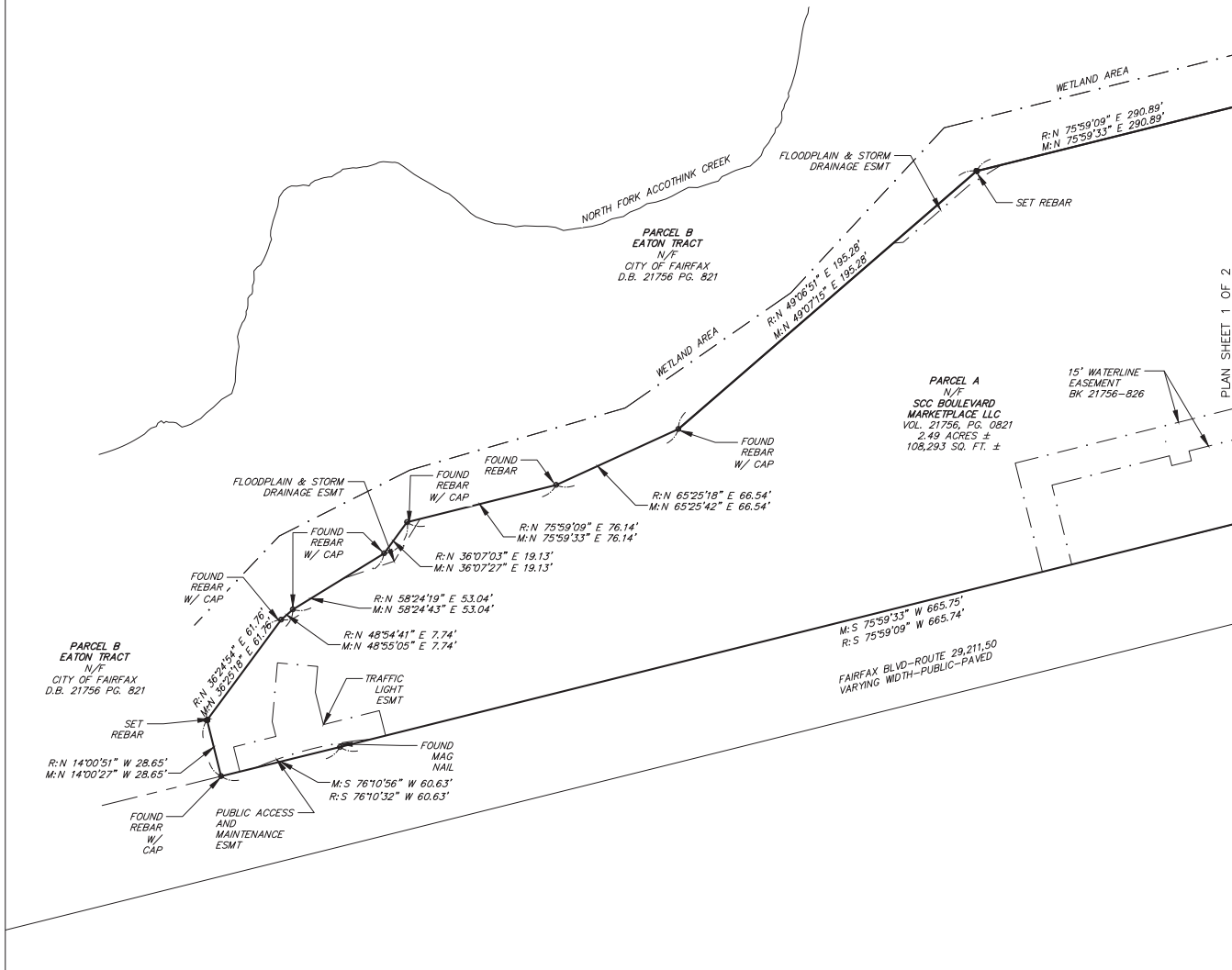
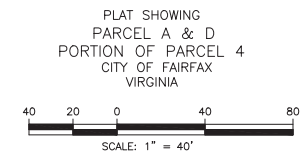
ZONING OFFICIAL

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DATE

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APPROVAL VOID IF PLAT IS NOT OFFERED FOR RECORD WITHIN 180 DAYS AFTER DATE THEREOF



REV:	BY:	DATE:	STATUS:
THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION. ANY ALTERATIONS, AUTHORIZED OR OTHERWISE, SHALL BE AT THE USER'S SOLE RISK.			

**Cornerstone**  
Energy Services

111 CHINQUAPIN ROUND ROAD  
SUITE 203  
ANNAPOLIS, MD 21401  
410-220-5615

EXISTING CONDITIONS SURVEY

OF: 10120 FAIRFAX BLVD  
FAIRFAX, VA

FOR: SOUTH PAW, DAVID PANELLA  
354 AMITY ROAD, SUITE 200  
WOODBRIIDGE, CT 06525

RECORD OWNER:  
JDC BOULEVARD, LLC  
C/O ROSENTHAL PROPERTIES

DRAWN	CHECKED
AR/MA	MJG
PROJECT NO.	DATE
22320.10	1/30/24
SHEET SIZE	SCALE
18'X24"	1"=40'
SHEET 2 OF 2	



**Board of Architectural Review**

DATE: March 6, 2024  
TO: Board of Architectural Review Chair and Members  
THROUGH: Jason Sutphin, Community Development Division Chief *JDS*  
FROM: Anna Kohlbrenner, BAR Liaison *AK*  
SUBJECT: **Work Session: 10120 Fairfax Blvd – Taco Bell**

- ATTACHMENTS: 1. Relevant Regulations  
2. Current plan set

**Nature of Request**

- 1. Case Number: BAR-24-00087
- 2. Address: 10120 Fairfax Blvd
- 3. Request: Restaurant with drive-through
- 4. Applicant: ABTB Mid-Atlantic LLC
- 5. Applicant’s Representative: Kathryn R. Taylor
- 6. Status of Representative: Attorney
- 7. Zoning: CR Commercial Retail, Architectural Control Overlay District

**BACKGROUND**

The subject property is located north of Fairfax Blvd, west of Patient First, and east of the ABC store. The applicant is proposing a 2,256 SF restaurant with a drive-through located at the Boulevard Marketplace Shopping Center. The applicant is proposing an amendment to a general development plan that was approved in conjunction with a rezoning in 2008. In the past, the current site location was approved for a drive-through bank in 2008 and a retail building in 2017, in which neither have been built at this time as the property remains vacant. The applicant proposes an amendment to the Zoning and General Development Plan and a Special Use Permit for a restaurant with drive-through. There was a pre-application meeting with the Planning Commission and City Council in early 2023. Comments included traffic/stacking concerns. Work sessions have not been scheduled at this time with the Planning Commission or the City Council.

**PROPOSAL**

The applicant is proposing a 2,256 SF restaurant with a drive-through lane with 13 stacking spaces, parking spaces, and an outdoor seating area. There is an existing walking path to the west of the site that would remain. Cars would enter the site along Fairfax Blvd then have the option to enter the drive-

through or park their car and walk into the proposed Taco Bell. The proposed building would be 28' wide and 42' in depth. The proposed brick would match brick located around the shopping center and would be in colors *burn almond*, *red cliff*, and *royal salisbury*. Downspouts and cornice trim would be in color *worldly gray* semi-gloss. Hardie trim, Hardie paneling, and metal parapet cap would be in color *cyberspace* semi-gloss. The applicant is proposing black metal canopies, purple Hardie panels, patio furniture, railing, and exterior artwork panels.

The applicant is working with the city's urban forester on the proposed landscaping. The BAR does not review outdoor dining furniture anymore, but does review railings, and other permanent structures associated. Rooftop mechanical would be fully hidden from view according to the applicants plan set.

## RELEVANT DESIGN GUIDELINES

### *City of Fairfax Design Guidelines:*

*The following excerpts from the Design Guidelines are relevant to this application.*

#### *New Construction, ACOD-3*

##### *Building Sitting, ACOD-3.2*

*Construct new buildings in the ACOD with setbacks to reinforce the street wall depending on the zoning classification and the use(s) of the building. If the new building contains retail uses that need limited parking in front of the building, consider facing the parking to the street and use lower plantings to partially screen vehicles. Taller plantings also can serve as screening and aid in creating a stronger street wall where there are spaces between buildings or sites.*

The rest of the site has already been developed, as parking is located in front of the proposed restaurant along Fairfax Blvd. The existing parking would remain.

##### *Building Orientation, ACOD-3.3*

*Buildings should be sited so that their main entrances are facing the street on which they are located.*

The main entrance faces Fairfax Blvd showing signage, a door, and outdoor dining area.

##### *Building Form & Articulation, ACOD-3.4*

*Use forms in new construction that relate to those of existing neighboring buildings on the street that are of quality design. Most have simple rectangular forms that may be horizontal or vertical depending on the number of stories of the building.*

Surrounding buildings are rectangular in nature.

*Building Scale, ACOD-3.5*

*Reinforce the human scale of new design in ACOD by including different materials, textures, or colors within a large building and/ or by dividing large facades and other elevations into different bays with different heights and planes. Use other techniques such as varying rooflines and window patterns, articulating entrances, and adding cornices and string and belt courses to separate floor levels, and using other decorative features. Corner articulation, balconies, canopies, marquees, and awnings can all also help create a human scale.*

The applicant is proposing different materials and textures throughout the building to break up the façade. The massing of the building along Fairfax Blvd is under 30' wide, but the applicant still breaks up the façade to show different heights and dimensions. The main architectural corner is higher than the rest of the building. Human scale is introduced by the implementation of an outdoor dining area.

*Roof Form & Materials, ACOD-3.6*

*The roof design of small infill commercial buildings should be sloped behind a parapet wall; or it may be a gable or hipped roof design.*

The roofline matches the surrounding shopping center and mechanical would be hidden from view. The cornice appears slightly out of proportion.

*Materials & Textures, ACOD-3.9*

*The selection of materials and textures for a new building in the ACOD may include brick, stone, cast stone, wood or cementitious siding, metal, glass panels, or other materials as deemed appropriate by Staff and the BAR. In general, the use of stucco-like products such as EIFS should be limited and is most appropriate on higher elevations, not in the pedestrian realm. Larger-scale buildings whose primary facades have been divided into different bays, planes, and heights to reduce their visual impact also may vary materials and textures as well. Use quality materials*

*consistently on all publicly visible sides of buildings in the district. These materials should be long lasting, durable, maintainable, and appropriate for environmental conditions. Avoid the use of aluminum or vinyl siding and plain concrete masonry units as exterior materials or painted metal siding. EIFS (artificial stucco) may be appropriate if used in small proportions above pedestrian level. Avoid the use of tinted, opaque, or mirrored glass for windows and doors in the district.*

The applicant is proposing high quality materials including three different colors of brick that directly matches the shopping complex. The applicant is not proposing any EIFs.

*Building-Mounted Lighting, ACOD-3.12*

*Lighting for new structures should be designed to be an integral part of the overall design by relating to the style, material, and/ or color of the building. Fixtures should utilize an incandescent, LED, fluorescent, metal halide, or color corrected high-pressure sodium lighting sources. Avoid overly bright or colored lights. Fixtures should be the full cutoff variety to limit the impact of lighting on neighboring properties. A combination of free-standing and wall-mounted fixtures is recommended to yield varied levels of lighting and to meet the intent of the zoning regulations.*

The applicant is proposing exterior wall sconces that are the same color and look as others in the complex. The proposed wall sconces would be downlight fixtures.

*Appurtenances, ACOD-3.13*

*Mechanical equipment on roofs or sides of buildings should not be visible from streets. It should be screened from public view on all sides if otherwise visible. The screening should be consistent with the design, textures, materials, and colors of the building. Another method is to place the equipment in a nonvisible location behind a parapet.*

Roof-top mechanical would be fully screened by the roofline and would not be visible from the ROW in any direction.

*Awnings & Canopies, ACOD-5*

*Materials & Color, ACOD-5.3*



*Coordinate color scheme of awnings and canopies with the overall building color scheme.*

Proposed canopies appear low-profile in nature and would be metal in color black.

*Private Site Design & Elements, ACOD-6*

*Parking, ACOD-6.2*

*Hide or screen parking from view of the public right-of-way by locating it within the building mass.*

Parking along Fairfax Blvd is already existing and landscaping is located in front of it.

*Landscaping, ACOD-6.3*

*Use landscape edges such as a row of street trees. Where trees cannot be installed due to utility or other restrictions, use a shrub layer or herbaceous planting to create a unifying edge or seam between adjacent developments and their face on the public right-of-way. Create well-defined outdoor spaces, delineate pathways and entries, and create a sense of continuity from one site to the next.*

The applicant is working with our urban forester and has provided a planting plan for review.

ATTACHMENT 1

## RELEVANT REGULATIONS

### §6.5.1. Applicability

Certificates of appropriateness shall be reviewed in accordance with the provisions of §6.5.

A. A certificate of appropriateness shall be required:

1. To any material change in the appearance of a building, structure, or site visible from public places (rights-of-way, plazas, squares, parks, government sites, and similar) and located in a historic overlay district (§3.7.2), the Old Town Fairfax Transition Overlay District (§3.7.3), or in the Architectural Control Overlay District (§3.7.4). For purposes of §6.5, “material change in appearance” shall include construction; reconstruction; exterior alteration, including changing the color of a structure or substantial portion thereof; demolition or relocation that affects the appearance of a building, structure or site;

### §6.5.3. Certificate of appropriateness types

A. Major certificates of appropriateness

1. Approval authority

(a) General

Except as specified in §6.5.3.B.2(b), below, the board of architectural review shall have authority to approve major certificates of appropriateness.

(b) Alternative (in conjunction with other reviews)

Alternatively, and in conjunction with special use reviews, planned development reviews, special exceptions or map amendments (rezoning), the city council may approve major certificates of appropriateness.

### §6.5.6. Action by decision-making body

A. General (involving other review by city council)

After receiving the director’s report on proposed certificates of appropriateness, which do not involve other reviews described below, the board of architectural review (BAR) shall review the proposed certificates of appropriateness in accordance with the approval criteria of §6.5.7. The BAR may request modifications of applications in order that the proposal may better comply with the approval criteria. Following such review, the BAR may approve, approve with modifications or conditions, or disapprove the certificate of appropriateness application, or it may table or defer the application.

B. Other reviews

1. Prior to taking action on special use reviews, planned development reviews, and map amendments (rezoning), the city council shall refer proposed certificates of appropriateness to the BAR for review in accordance with the approval criteria of §6.5.7.
2. In conjunction with special use reviews, planned development reviews, special exceptions and map amendments (rezoning), the city council may review the proposed certificate of appropriateness in accordance with the approval criteria of §6.5.7. The city council may request

modifications of applications in order that the proposal may better comply with the approval criteria. Following such review, the city council may approve, approve with modifications or conditions, or disapprove the certificate of appropriateness application, or it may table or defer the application.

#### §6.5.7. Approval criteria

##### A. General

1. Certificate of appropriateness applications shall be reviewed for consistency with the applicable provisions of this chapter, any adopted design guidelines, and the community appearance plan.
2. Approved certificates of appropriateness shall exhibit a combination of architectural elements including design, line, mass, dimension, color, material, texture, lighting, landscaping, roof line and height conform to accepted architectural principles and exhibit external characteristics of demonstrated architectural and aesthetic durability.

#### §6.5.9. Action following approval

- A. Approval of any certificate of appropriateness shall be evidenced by issuance of a certificate of appropriateness, including any conditions, signed by the director or the chairman of the board of architectural review. The director shall keep a record of decisions rendered.
- B. The applicant shall be issued the original of the certificate, and a copy shall be maintained on file in the director's office.

#### §6.5.10. Period of validity

A certificate of appropriateness shall become null and void if no significant improvement or alteration is made in accordance with the approved application within 18 months from the date of approval. On written request from an applicant, the director may grant a single extension for a period of up to six months if, based upon submissions from the applicant, the director finds that conditions on the site and in the area of the proposed project are essentially the same as when approval originally was granted.

#### §6.5.11. Time lapse between similar applications

- A. The director will not accept, hear or consider substantially the same application for a proposed certificate of appropriateness within a period of 12 months from the date a similar application was denied, except as provided in §6.5.11.B, below.
- B. Upon disapproval of an application, the director and/or board of architectural review may make recommendations pertaining to design, texture, material, color, line, mass, dimensions or lighting. The director and/or board of architectural review may again consider a disapproved application if within 90 days of the decision to disapprove the applicant has amended his application in substantial accordance with such recommendations.

#### §6.5.12. Transfer of certificates of appropriateness

Approved certificates of appropriateness, and any attached conditions, run with the land and are not affected by changes in tenancy or ownership.

§6.5.13. Appeals

A. Appeals to city council

Final decisions on certificates of appropriateness made may be appealed to city council within 30 days of the decision in accordance with §6.22.

B. Appeals to court

Final decisions of the city council on certificates of appropriateness may be appealed within 30 days of the decision in accordance with §6.23.

**ABTB Mid-Atlantic, LLC**

**STATEMENT OF INTENT**

**January 18, 2024**

Please accept the following as a Statement of Intent in support of the submitted Major Certificate of Appropriateness application to allow for the development of 10120 Fairfax Boulevard with an approximately 2,256 square foot restaurant with a drive-through window/facility. This Statement of Intent is submitted in conjunction with the Certificate of Appropriateness design package, consisting of thirty-one (31) sheets, and other submitted supporting materials. The contents of this Statement of Intent address the approval considerations for certificates of appropriateness, as set forth in Section 6.5.7.A of the City of Fairfax Zoning Ordinance (the “Zoning Ordinance”).

ABTB Mid-Atlantic, LLC (the “Applicant”) is the contract lessee of an approximately 0.66 acre property located in the City of Fairfax. The property consists of one (1) tax parcel identified among the City of Fairfax’s tax assessment records as 47-4-23-000-D (the “Subject Property”). The Subject Property is currently a vacant pad site that is part of the Boulevard Marketplace Shopping Center (the “Shopping Center”). Zoned to the CR (Commercial Retail) District, the Subject Property is included in a Generalized Development Plan (“GDP”) that was approved in conjunction with a rezoning in 2008, allowing a total of 17,300 square feet of commercial space, including a 4,300 square foot bank building with drive-through on the Subject Property. In 2011, the Shopping Center was subdivided into two separate parcels, including the Subject Property and the adjacent parcel identified as 47-4-23-000-A (“Parcel A”). In 2017, City Council approved a GDP amendment, which converted the previously approved drive-through bank on the Subject Property to an approximately 5,100 square foot retail building. To date, the approved retail building has not been constructed, and the Subject Property remains vacant.

**PROPOSED DEVELOPMENT**

The Applicant proposes to redevelop the Subject Property with a drive-through Taco Bell restaurant. No modifications or improvements are proposed to Parcel A nor its existing buildings. The proposed drive-through restaurant is a commercial use that is consistent with the recommendations of the Comprehensive Plan and compatible with the commercial character of the surrounding area. The proposed restaurant will consist of an approximately 2,256 square-foot, one story building with a proposed parking area oriented towards the rear of the Subject Property. The proposed drive-through lane will wrap around the new building and proposed parking area. In addition, bicycle parking and a small outdoor dining patio area will be provided proximate to the restaurant’s main entrance. As shown on the submitted materials, the proposed restaurant building will be architecturally compatible with the adjacent buildings in the Shopping Center and will be constructed of high-quality materials.

## CITY OF FAIRFAX DESIGN GUIDELINES

As illustrated in the submitted materials, the overall design of the proposed development is consistent with the City of Fairfax Design Guidelines. The proposed development is characterized by the installation of attractive architecture, the provision of quality landscaping, and a reduction of impervious surface. To ensure compatibility with the existing design of the Shopping Center, the proposed restaurant will include the use of masonry and cornice details that are consistent with the materials of the two existing buildings on Parcel A. Building materials may include, but are not limited to brick, Hardie cement board panels, E.I.F.S, and other quality materials. The proposed restaurant building will be a maximum height of twenty-five (25) feet. As such, the mass, scale, height, and architectural style of the proposed building will be compatible and proportionate to the remainder of the Shopping Center.

Quality landscaping is also proposed, including a mix of shade, ornamental, and evergreen trees in combination with evergreen and deciduous shrubs as well as ground cover, perennials, and ornamental grasses along the perimeter of the parking lot and the drive-through lane. The proposed landscaping will provide more vegetation on-site compatible to what is provided on the parcels adjacent to the Subject Property. In addition to the proposed landscaping, the Applicant is proposing to retain all of the existing street trees and other landscaping along Fairfax Boulevard. In the end, the proposed development will result in increased open space and an overall decrease of impervious surface.

The proposed site lighting is shown on Sheets 7 and 8 of the design package. All proposed site lighting will comply with Zoning Ordinance requirements. Lighting surrounding the building will consist of LED lighting on 25'-0" bronze posts and LED fixtures, consistent with the Zoning Ordinance. All light fixtures will be located and aimed to minimize glare and stray light trespassing across lot lines and into the public right-of-way. All of the existing street lights along Fairfax Boulevard will remain. Overall, the Applicant's lighting plan will illuminate the proposed parking lot and pedestrian pathways to provide safe vehicular and pedestrian circulation as well as minimize pedestrian/vehicular conflicts.

As to additional appurtenances associated with the proposed development, the Applicant has included an enclosed dumpster that will be located in the northern area of the proposed parking lot on the Subject Property. The proposed dumpsters will be screened by a combination of evergreen trees, evergreen shrubs, and a shade tree surrounding a brick enclosure with painted solid metal gates to match the building in order to minimize visual impact. Additionally, bicycle parking is proposed in accordance with the Zoning Ordinance requirements. Lastly, any rooftop mechanical equipment installed on the roof of the proposed restaurant building will be screened.

The proposed improvements will encourage and foster enhanced pedestrian connectivity within the site as well as with the surrounding community. To accommodate adequate pedestrian circulation, the Applicant will incorporate a 6-foot wide sidewalk between the proposed building and parking areas. In addition, the Applicant proposes to maintain an existing 8-foot wide trail along the western property line that connects to the George Snyder Trail to the north of the site. A 3.5-wide landscape strip is proposed between the drive-through lane and the 8-foot wide trail as shown on the submitted materials of the design package to provide an additional vegetated buffer

between pedestrians and vehicles in the drive-through lane. These new sidewalks and connections will contribute to the enhancement of the pedestrian experience along this stretch of the Fairfax Boulevard corridor.

In conclusion, the Applicant's proposal presents an opportunity to redevelop and activate an underutilized parcel with a vibrant, high-quality community-serving use that advances the Comprehensive Plan's objectives of enhancing commercial activities along the Fairfax Boulevard commercial corridor. The proposed site design consists of high-quality architecture, landscaping, and pedestrian improvements that are consistent with the City's Design Guidelines and will enhance the quality of one of the City's primary commercial corridors. The Applicant is eager to bring the first Taco Bell to the City and is committed to ensuring that the proposed development fits into the fabric of Fairfax.



**10120 Fairfax Boulevard**

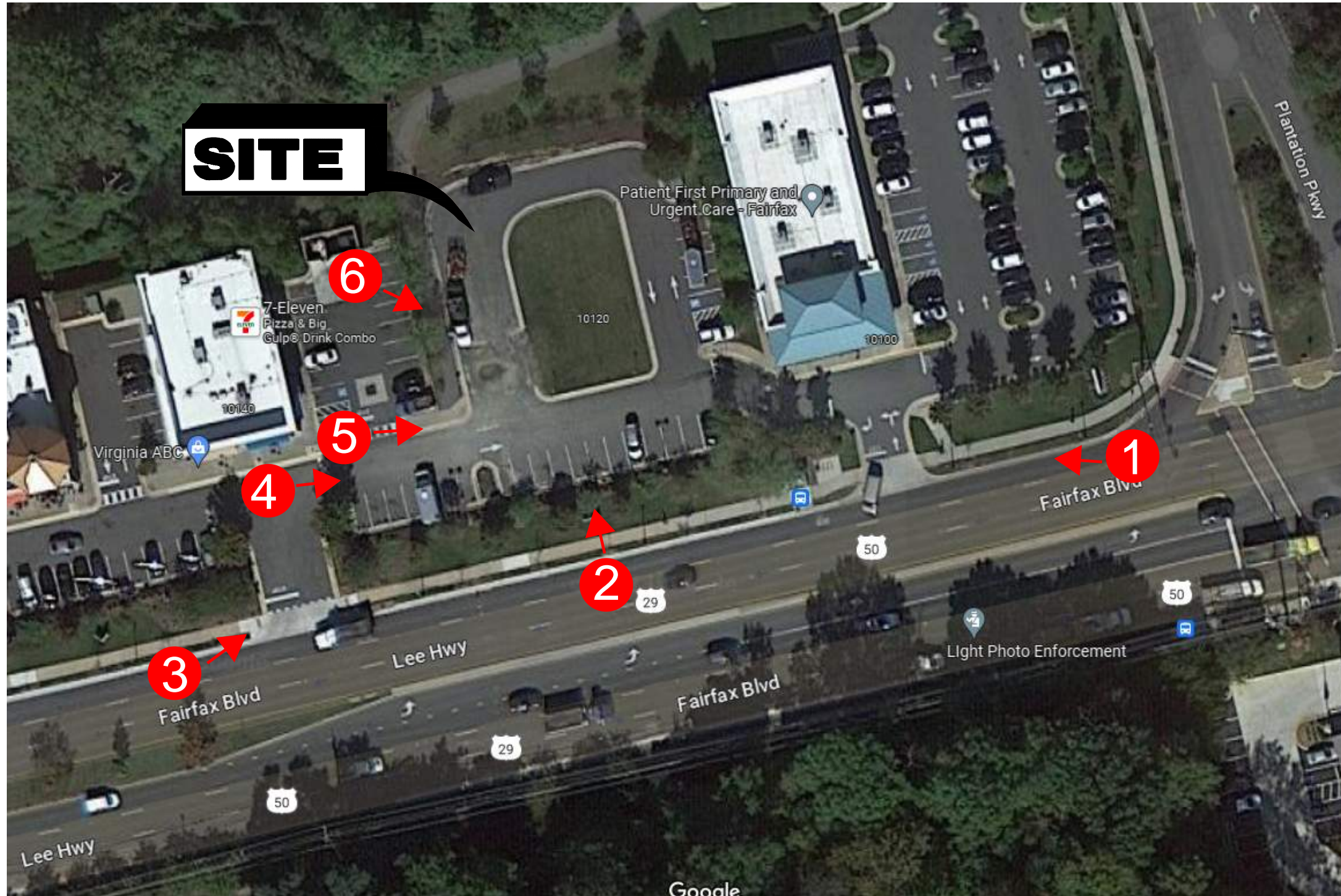
# **Board of Architectural Review Certificate of Appropriateness**

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1	Cover Sheet
2	Existing Conditions Aerial
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4	GDP/SUP Plat Site Plan
5	Landscape Plan
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EXISTING CONDITIONS AERIAL





**1** Fairfax Blvd: Existing East Entrance



**2** Fairfax Blvd: Front of Site



**3** Fairfax Blvd: Existing West Entrance



**4** 7-Eleven Parking Lot: West



**5** 7-Eleven Parking Lot: West

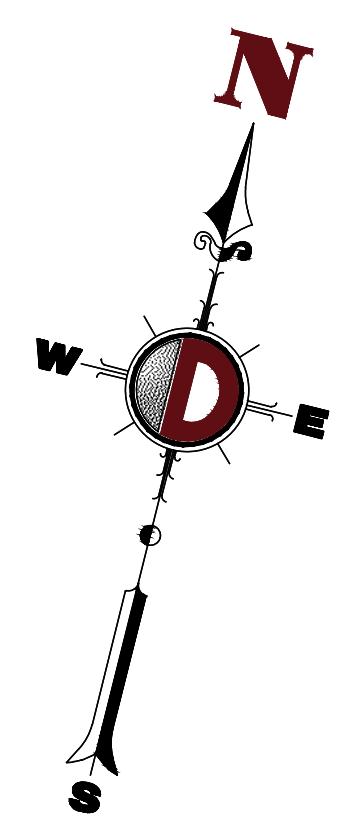
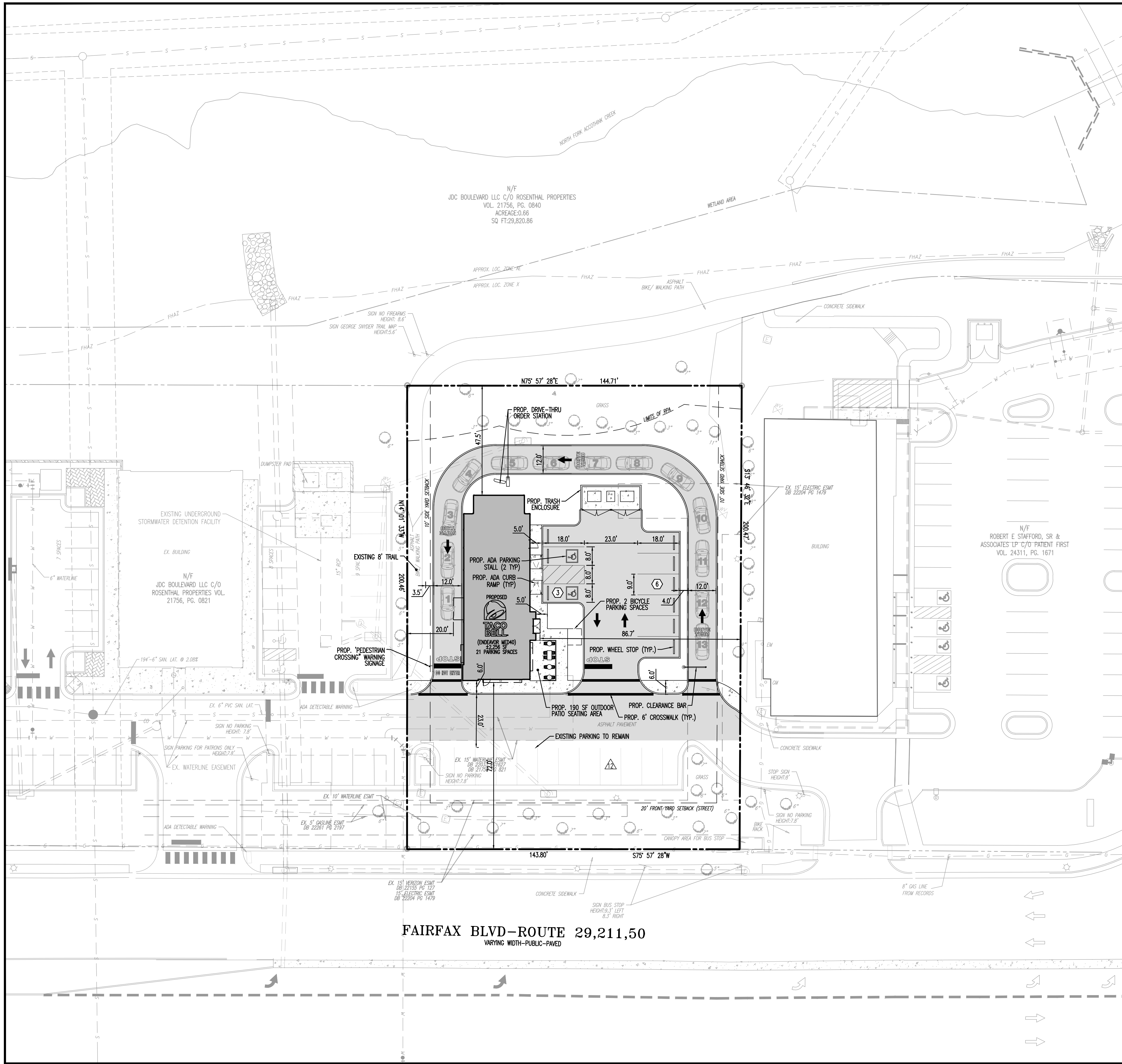


**6** 7-Eleven Parking Lot: West

EXISTING CONDITIONS PHOTO EXHIBIT



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 File: \\deep\local\defenders\Data\DEFC PROJECTS\3486 A8D0 Capital\22-01538 Fairfax VA\DW\Special Exception Plans\034862201538S0.dwg, ---> 03 GENERAL DEVELOPMENT PLAN & SPECIAL USE PERMIT PLAN



**ZONING TABULATIONS**

TAX MAP NUMBER	47-4-23-000-0, 47-4-23-000-A (NO MODIFICATIONS ARE PROPOSED TO PARCEL A AND ALL EXISTING IMPROVEMENTS ARE TO REMAIN)
PROPERTY AREA	0.66 AC (PARCEL D)
EXISTING ZONING	CR (COMMERCIAL RETAIL)
EXISTING USE	VACANT (PARCEL D)
PROPOSED USE	FAST FOOD RESTAURANT WITH DRIVE THROUGH (PARCEL D)
COMP PLAN LAND USE	CC (COMMERCIAL CORRIDOR)
BUILDING AREA	2,256 SF (PARCEL D)

**ZONING COMPLIANCE (PARCEL D)**

	ALLOWED/REQUIRED	PROVIDED
LOT AREA; MINIMUM	20,000 SF	28,921 SF
YARD REQUIREMENTS		
FRONT SETBACK; STREET (FAIRFAX BLVD)	93' MAXIMUM, 20' MINIMUM	73.0'
SIDE SETBACK (EAST)	10'	88.6'
SIDE SETBACK (WEST)	10'	24.5'
REAR SETBACK (NORTH)	0'	47.4'
BULK PLANE REQUIREMENTS	N/S	N/S
LOT WIDTH MINIMUM	N/S	N/S
BUILDING HEIGHT	5 STORES; 60 FT	1 STORY; 25 FT
BUILDING COVERAGE; MAXIMUM	60%	7.80%
LOT COVERAGE; MAXIMUM	85%	39.31%
FLOOR AREA; MAXIMUM	N/S	N/S
PARKING AND LOADING REQUIREMENTS (1 SPACE / 200 SF OF FLOOR AREA)	10	21
LOADING	NONE FOR BUILDINGS UNDER 10,000 SF	NONE
STACKING REQUIREMENT	10	13

**STORMWATER MANAGEMENT NARRATIVE**  
 THE EXISTING PARCEL IS THE EASTERN PORTION OF A LARGER OVERALL COMMERCIAL DEVELOPMENT, "BOULEVARD MARKETPLACE." IN THE CURRENT CONDITION, A VACANT GRASS COVERED BUILDING PAD IS SURROUNDED BY EXISTING PAVING, PARKING AREAS AS WELL AS LANDSCAPING ON ALL SIDES OF THE DEVELOPMENT. THERE IS AN EXISTING RESOURCE PROTECTION AREA (RPA) BOUNDARY TO THE IMMEDIATE NORTH OF THE SITE THAT IS NOT CURRENTLY BEING IMPACTED, OR PROPOSED TO BE IMPACTED BY DEVELOPMENT.

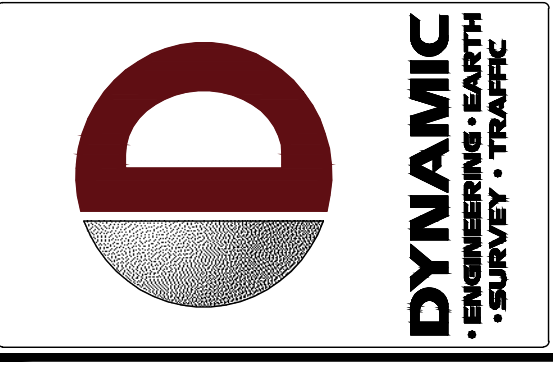
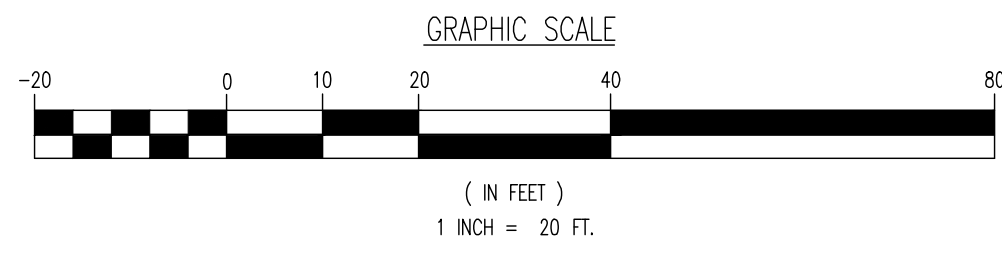
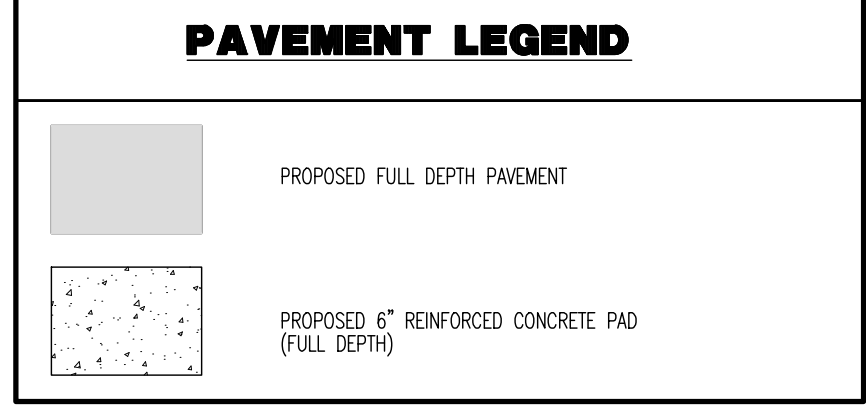
AS A PART OF THE PREVIOUS SHOPPING CENTER CONSTRUCTION, AN EXISTING UNDERGROUND STORM FILTER AND DETENTION FACILITY WAS INSTALLED (2011 SITE PLAN AMENDMENT NO. 1190151) AND CURRENTLY TREATS THE PARCEL OF INTEREST, IN ADDITION TO THE NEIGHBORING PARCEL TO THE WEST, WHICH IS GRANDFATHERED FROM THE 2014 STORMWATER MANAGEMENT REGULATIONS.

CONFIRMED THROUGH SURVEY AND PER THE PREVIOUS SITE PLAN SP-1704063, BMP REQUIREMENTS ARE MET THROUGH THE INSTALLED CONTECH STORMFILTER STRUCTURE, AS WELL AS PROVIDING THE NECESSARY STORM DETENTION TO RESTORE RUNOFF VALUES TO PRE-DEVELOPMENT RATES. THE EXISTING SITE DRAINAGE OUTFALLS THROUGH STORM DRAIN TO ACCOTINK CREEK. MOST OF ALL SITE DRAINAGE IS CONTAINED TO ON-SITE STORM DRAIN WITH LITTLE SHEET FLOW OFF SITE. NO MODIFICATIONS TO THIS EXISTING STRUCTURE ARE PROPOSED, AND THE PROPOSED SITE CONSTRUCTION/GRADING FOR THE NEW DRIVE THROUGH RESTAURANT WILL UTILIZE THE EXISTING STORMWATER INFRASTRUCTURE WITH LITTLE MODIFICATION TO THE EXISTING GRADING AND DRAINAGE PATTERNS.

PER THE PREVIOUS DESIGN, THE CONTECH DEVICE ON SITE WAS INSTALLED IN ORDER TO TREAT IN EXCESS OF APPROXIMATELY 13,633 SQUARE FEET OF IMPERVIOUS AREA. THE NEW DESIGN, WITH A SMALLER PROPOSED BUILDING FOOTPRINT AND ADDITIONAL GREENSPACE, CREATES 11,370 SQUARE FEET OF IMPERVIOUS AREA. WITH A REDUCTION OF CLOSE TO 2,000 SF OF TOTAL IMPERVIOUS AREA, THE EXISTING CONTECH DEVICE ON SITE WILL STILL MEET AND EXCEED BMP AND STORMWATER STORAGE REQUIREMENTS PER THE PREVIOUS APPROVAL CRITERIA. IN ADDITION, THE SITE LIMITS DISTURBANCE TO THE ESTABLISHED VEGETATION ALONG THE EXISTING CURB LINE AND CREATES NO ADDITIONAL GRADING OR MODIFICATION TO AREAS INFRRING ON THE EXISTING RPA.

**WATER AND SEWER NARRATIVE**  
 THE SITE PRESENTLY HAS AN EXISTING WATER SERVICE CONNECTION AND OUTSIDE WATER METER SERVICED VIA THE EXISTING 6" WATER LATERAL ON THE NEIGHBORING PROPERTY TO THE WEST. THE SITE SIMILARLY HAS AN EXISTING SANITARY SERVICE CONNECTION ALSO SERVICED VIA AN EXISTING 6" SANITARY LATERAL ON THE NEIGHBORING PROPERTY TO THE WEST. PROPOSED WATER AND SEWER SERVICE CONNECTIONS TO THE NEW RESTAURANT BUILDING WILL BE PROVIDED AS PART OF THIS DEVELOPMENT, UTILIZING THE EXISTING CONNECTION POINTS.

FINAL DESIGN COORDINATION WILL BE DONE WITH THE APPROPRIATE WATER AND SEWER AGENCIES AS PART OF THE SITE PLAN PROCESS.



REV.	DATE	PER LANDLORD COMMENTS
1	12/22/23	

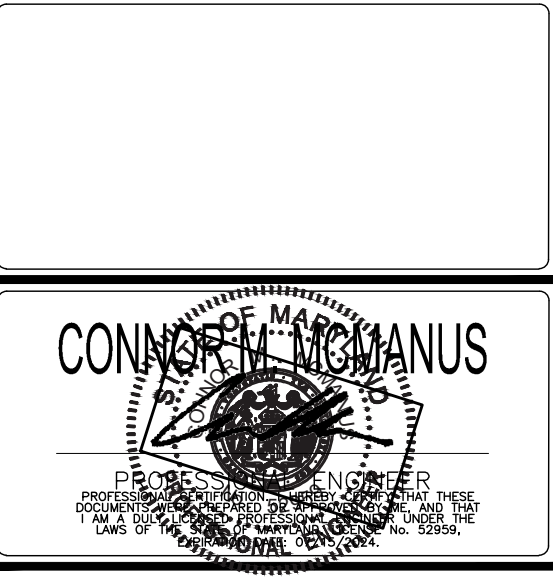
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DESIGNED BY: SRM  
 CHECKED BY: CM  
 DRAWN BY: JJ

PROJECT: SOUTHPIK PROPOSED TACO BELL FAST FOOD RESTAURANT W/ DRIVE-THRU BOULEVARD MARKETPLACE; PARCEL D 10120 FAIRFAX BOULEVARD CITY OF FAIRFAX, FAIRFAX COUNTY, VIRGINIA

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TITLE: GENERAL DEVELOPMENT PLAN & SPECIAL USE PERMIT PLAN

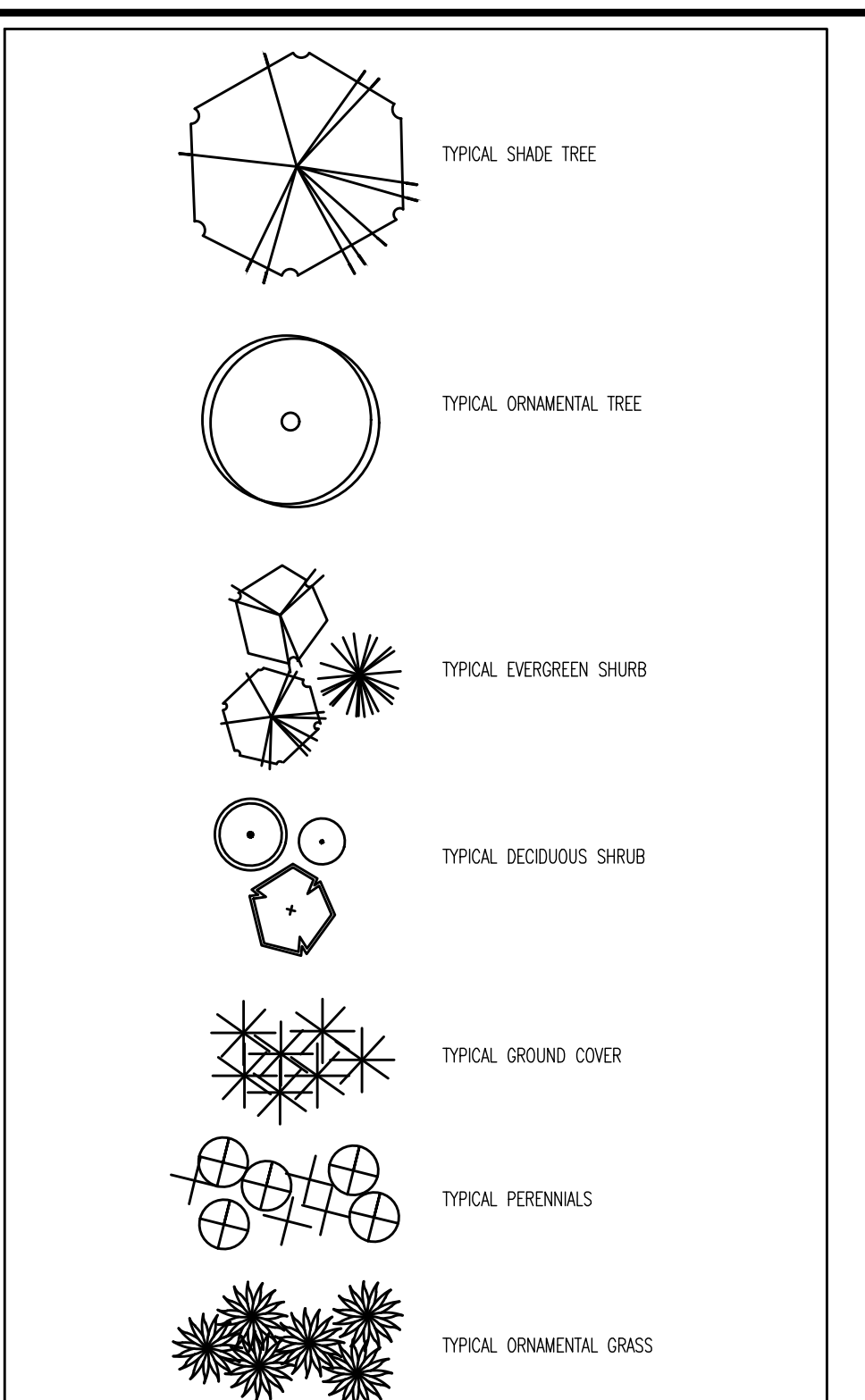
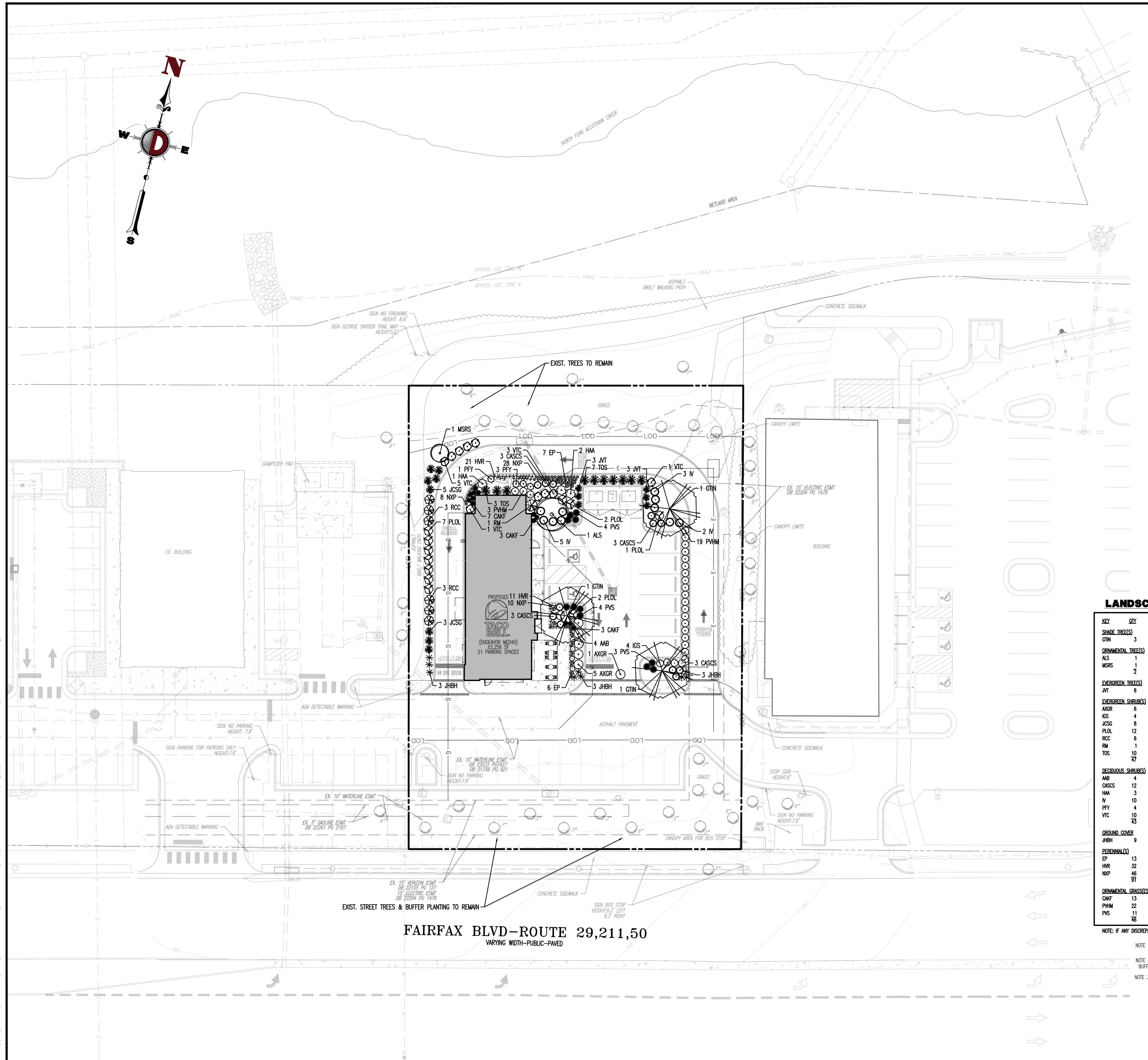
SCALE: (H) AS NOTED DATE: 07/12/2023

PROJECT No: 3486-22-01538

SHEET No: 4 Rev. #: 1



Plotted: 12/22/23 - 1:18 PM, By: ebel  
 File: \\deepa\local\defenders\Data\DEFC PROJECTS\3486 A8D0 Capital\22-01538 Fairfax VA\DW\Special Exception Plans\03486210138SL0.dwg, ----> 05 CONCEPTUAL LANDSCAPE PLAN AND TABULATIONS



**SITE TREE COVERAGE REQUIREMENTS**

**TREE CANOPY CALCULATIONS**  
 TOTAL SITE AREA = 28,921 SF  
 - TREE CANOPY REQUIRED (10%) = 2,892 SF  
 - TREE CANOPY PROVIDED = 5,425 SF (18.8%)

- (A) PROPOSED SEVEN (7) SHADE TREES @ 250 SF OF EACH = 250\*7 = 750 SF
- (B) EXISTING SEVEN (7) SHADE TREES @ 250 SF OF EACH = 250\*7 = 2,550 SF
- (A) PROPOSED TWO (2) ORNAMENTAL TREES @ 125 SF OF EACH = 125\*2 = 250 SF
- (B) EXISTING FIFTEEN (15) ORNAMENTAL TREES @ 125 SF OF EACH = 125\*15 = 1,875 SF

**STREET TREES**

BOUNDARY	LENGTH	REQUIRED TREES	PROVIDED TREES
FAIRFAX BOULEVARD	143.8 LF	4 TREES	5 TREES

NOTE: 5 PROPOSED TREES AND 22 EXISTING TREES INCLUDED IN THE CALCULATIONS

**INTERIOR PARKING LOT**

NUMBER OF PROVIDED SPACES	21 SPACES
REQUIRED TREES	3 (21/10 = 2.1)
PROVIDED TREES	3

**LANDSCAPE SCHEDULE**

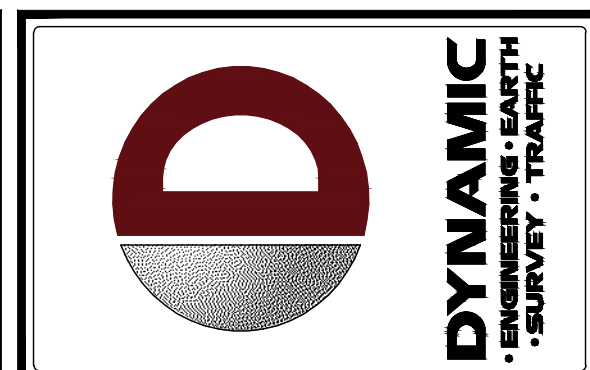
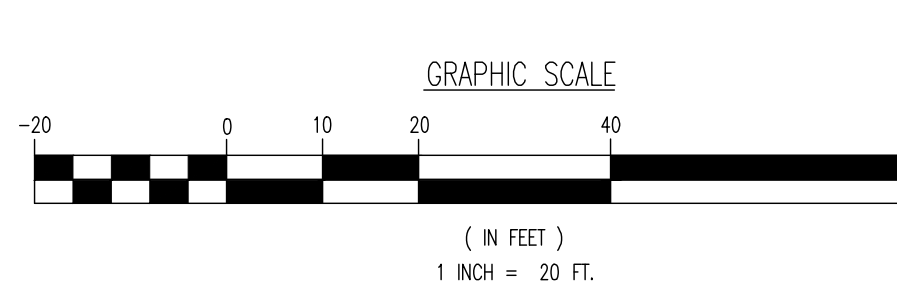
KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE	REMARKS
<b>SHADE TREE(S)</b>					
GTN	3	GLEDITSIA TRICANTHOS 'SUNCOLE'	SUNBURST THORNLESS HONEYLOCUST	2 1/2-3' CAL.	B#B
<b>ORNAMENTAL TREE(S)</b>					
ALS	1	AMELANCHIER LAEVIS 'SNOWCLOUD'	SNOWCLOUD SERVICEBERRY	8-10'	B#B
MSRS	1	MAGNOLIA STELLATA 'ROYAL STAR'	ROYAL STAR MAGNOLIA	5-6'	B#B
<b>EVERGREEN TREE(S)</b>					
JVT	6	JUNIPERUS VIRGINIANA 'TAYLOR'	TAYLOR JUNIPER	5-6'	B#B
<b>EVERGREEN SHRUB(S)</b>					
AXGR	6	ABELIA X GRANDIFLORA 'RADANCE'	RADANCE ABELIA	24-30"	#3 CAN
IGS	4	ILEX GLABRA 'SHAMROCK'	SHAMROCK HEDGEHERRY HOLLY	24-30"	#3 CAN
JCSG	6	JUNIPERUS CHINENSIS 'SEA GREEN'	SEA GREEN JUNIPER	24-30" SPRD.	B#B
PLOL	12	PRUNUS LAUROCENSIS 'OTTO LUXEM'	OTTO LUXEM CHERRYLAUREL	24-30"	#3 CAN
RCC	6	RHODODENDRON CATAMBENSE 'RHONIDES'	RHONIDES RHODODENDRON	24-30"	B#B
RM	1	RHODODENDRON MAXIMUM 'ROSEMARY'	ROSEMARY RHODODENDRON	30-36"	#5 CAN
TOS	10	THUJA OCCIDENTALIS 'SMARAGD'	EMERALD GREEN ARBORVITAE	5-6'	B#B
<b>DECIDUOUS SHRUB(S)</b>					
AAB	4	ARONIA MELANOCORPA 'UCONNAMI185'	LOW SCAPE MOUND CHOKEBERRY	18-24"	#3 CAN
CASCS	12	CLETHRA ALNIFOLIA 'SWEETEN CANDLES'	SWEETEN CANDLES SUMMERSWEET	24-30"	#3 CAN
HWA	3	HYDRANGEA HYDRANGIS 'ANNHELLE'	'ANNHELLE' HYDRANGEA	24-30"	#3 CAN
N	10	ITEA VIRGINICA 'HENRY'S GARNET'	GARNET SWEETSPIRE	30-36"	#5 CAN
PFY	4	POTENTILLA FRUTICOSA 'HAPPY FACE YELLOW'	HAPPY FACE YELLOW POTENTILLA	18-24"	#3 CAN
VTC	10	VEURINIUM TRILOBUM 'COMPACTUM'	COMPACT AMERICAN CRANBERRYBUSH	24-30"	#3 CAN
<b>GROUND COVER</b>					
JHBH	9	JUNIPERUS HORIZONTALIS 'BAR HARBOR'	BAR HARBOR CREEPING JUNIPER	15-18" SPRD.	#3 CAN
<b>PERENNIAL(S)</b>					
EP	13	ECHINACEA PURPUREA	PURPLE CONEFLOWER	2 GAL.	CONTAINER
HVR	32	HEMEROCALLIS VARIETES	PEACH-RED-YELLOW DAYLILY	1 GAL.	CONTAINER
NXP	46	NEPETA X 'PSYCHE'	LITTLE TRUPLY CATMINT	1 GAL.	#1
<b>ORNAMENTAL GRASS(S)</b>					
CAKF	15	CAJAMAGROSTIS ARUNDINACEA 'HAR. FROESTER'	FEATHER REED GRASS	2 GAL.	CONTAINER
PVHM	22	PANICUM VIRGATUM 'HEAVY METAL'	HEAVY METAL SWITCH GRASS	2 GAL.	CONTAINER
PVS	11	PANICUM VIRGATUM 'SHENANDOAH'	SHENANDOAH SWITCH GRASS	2 GAL.	CONTAINER

NOTE: IF ANY DISCREPANCIES OCCUR BETWEEN AMOUNTS SHOWN IN THE PLAN AND THE PLANT LIST, THE PLAN SHALL OBTAIN.

NOTE 1: DETAILED LANDSCAPING PLANTINGS, HARDSCAPE DESIGN, AND LIGHTING INFORMATION WILL BE PROVIDED AT TIME OF SITE PLAN.

NOTE 2: DUMPSTER ENCLOSURE SHOULD BE CONSTRUCTED OF CONCRETE BLOCK W/ DUR-O-WALL FINISH AND INCLUDES AN EVERGREEN BUFFER AROUND THE EXTERIOR.

NOTE 3: PER ZONING ORDINANCE SECTION 4.5.5.C.1 NO TRANSFER YARDS ARE REQUIRED TO ADJACENT CR AND/ OR I DISTRICT LOTS.



REV.	DATE	COMMENTS
1	12/22/23	PER LANDLORD COMMENTS

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DESIGNED BY: SRM  
 CHECKED BY: JJJ  
 C.M.  
 DRAWN BY: SRM

PROJECT: SOUTHAP PROPOSED TACO BELL FAST FOOD RESTAURANT W/ DRIVE-THRU BOULEVARD MARKETPLACE, PARCEL D 10120 FAIRFAX BOULEVARD CITY OF FAIRFAX, FAIRFAX COUNTY, VIRGINIA

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**CONCEPTUAL LANDSCAPE ARCHITECT**  
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TITLE: CONCEPTUAL LANDSCAPE PLAN AND TABULATIONS

SCALE: (H) AS NOTED DATE: 07/12/2023

PROJECT No: 3486-22-01538

SHEET No: 5 Rev. #: 1

OF 12 1













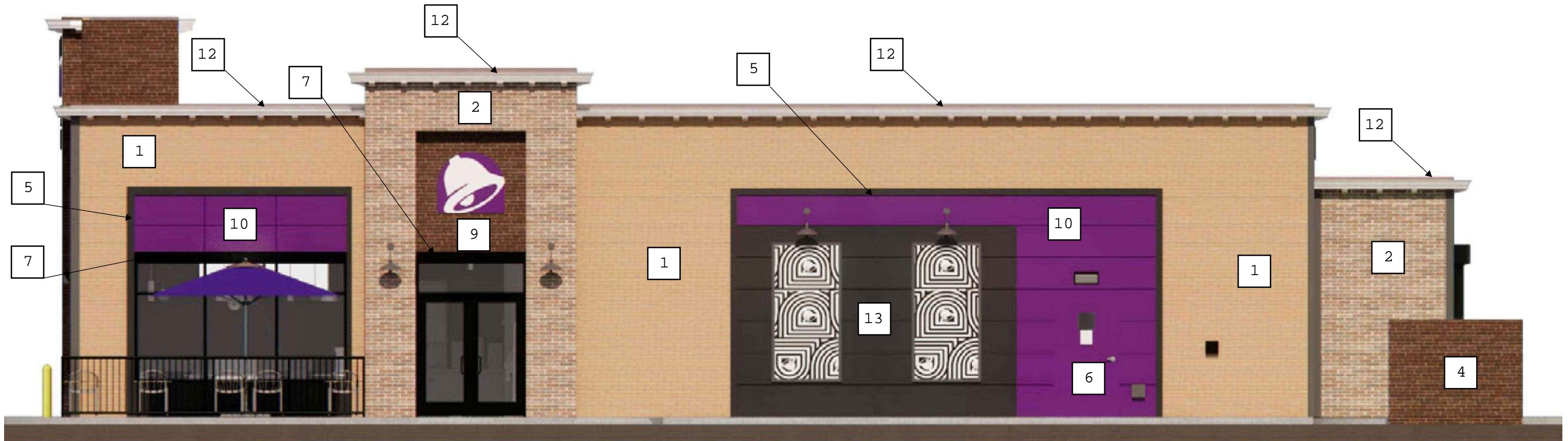




SYMBOL	AREA	MANUFACTURER	MATERIAL SPEC.	COLOR
1	FIELD BRICK	GLEN GERY	BRICK	BURN ALMOND
2	ACCENT BRICK	GLEN GERY	BRICK	RED CLIFF
3	DOWNSPOUTS	-	-	SHERWIN WILLIAMS SW7043 "WORLDLY GRAY", SEMI-GLOSS
4	ACCENT BRICK	TAYLOR	BRICK	ROYAL SALISBURY
5	HARDIE TRIM	JAMES HARDIE	HARDIE TRIM 5/4 SMOOTH 1" x 5 1/2"	SHERWIN WILLIAMS SW7076 "CYBERSPACE", SEMI-GLOSS
6	HOLLOW METAL DOOR	-	-	SW PURPLE TB2603C, SEMI-GLOSS
7	AWNINGS	SIGNAGE VENDOR	-	BLACK BY THE SIGNAGE VENDOR
8	CORNER TOWER	TAYLOR	BRICK	ROYAL SALISBURY
9	RECESS OF SIDE ENTRY PORTAL	TAYLOR	BRICK	ROYAL SALISBURY
10	HARDIE PANEL	JAMES HARDIE	PANEL SYSTEM SMOOTH	SW PURPLE TB2603C, SEMI-GLOSS
11	METAL PARAPET CAP	-	24GA GALVANIZED	SHERWIN WILLIAMS SW7076 "CYBERSPACE" KYNAR 500 COATING
12	CORNICE TRIM	-	-	SHERWIN WILLIAMS SW7043 "WORLDLY GRAY", SEMI-GLOSS
13	HARDIE PANEL	JAMES HARDIE	PANEL SYSTEM SMOOTH	SHERWIN WILLIAMS SW7076 "CYBERSPACE", SEMI-GLOSS

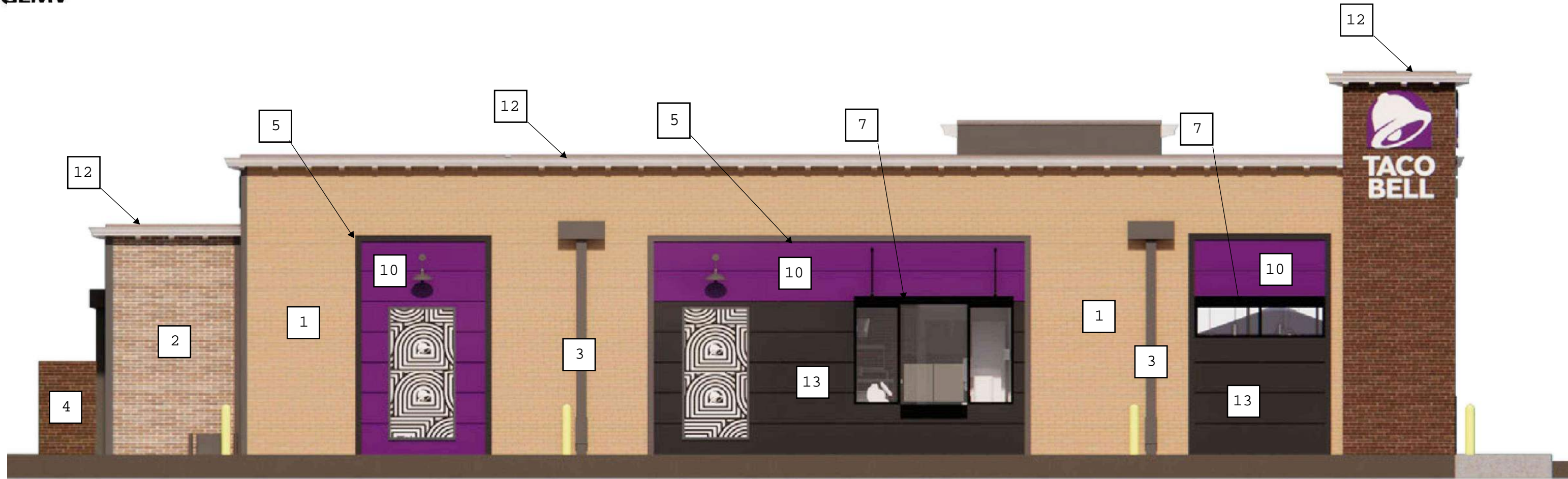
SOUTH ELEVATION





SYMBOL	AREA	MANUFACTURER	MATERIAL SPEC.	COLOR
1	FIELD BRICK	GLEN GERY	BRICK	BURN ALMOND
2	ACCENT BRICK	GLEN GERY	BRICK	RED CLIFF
3	DOWNSPOUTS	-	-	SHERWIN WILLIAMS SW7043 "WORLDLY GRAY", SEMI-GLOSS
4	ACCENT BRICK	TAYLOR	BRICK	ROYAL SALISBURY
5	HARDIE TRIM	JAMES HARDIE	HARDIE TRIM 5/4 SMOOTH 1" x 5 1/2"	SHERWIN WILLIAMS SW7076 "CYBERSPACE", SEMI-GLOSS
6	HOLLOW METAL DOOR	-	-	SW PURPLE TB2603C, SEMI-GLOSS
7	AWNINGS	SIGNAGE VENDOR	-	BLACK BY THE SIGNAGE VENDOR
8	CORNER TOWER	TAYLOR	BRICK	ROYAL SALISBURY
9	RECESS OF SIDE ENTRY PORTAL	TAYLOR	BRICK	ROYAL SALISBURY
10	HARDIE PANEL	JAMES HARDIE	PANEL SYSTEM SMOOTH	SW PURPLE TB2603C, SEMI-GLOSS
11	METAL PARAPET CAP	-	24GA GALVANIZED	SHERWIN WILLIAMS SW7076 "CYBERSPACE" KYNAR 500 COATING
12	CORNICE TRIM	-	-	SHERWIN WILLIAMS SW7043 "WORLDLY GRAY", SEMI-GLOSS
13	HARDIE PANEL	JAMES HARDIE	PANEL SYSTEM SMOOTH	SHERWIN WILLIAMS SW7076 "CYBERSPACE", SEMI-GLOSS

EAST ELEVATION



SYMBOL	AREA	MANUFACTURER	MATERIAL SPEC.	COLOR
1	FIELD BRICK	GLEN GERY	BRICK	BURN ALMOND
2	ACCENT BRICK	GLEN GERY	BRICK	RED CLIFF
3	DOWNSPOUTS	-	-	SHERWIN WILLIAMS SW7043 "WORLDLY GRAY", SEMI-GLOSS
4	ACCENT BRICK	TAYLOR	BRICK	ROYAL SALISBURY
5	HARDIE TRIM	JAMES HARDIE	HARDIE TRIM 5/4 SMOOTH 1" x 5 1/2"	SHERWIN WILLIAMS SW7076 "CYBERSPACE", SEMI-GLOSS
6	HOLLOW METAL DOOR	-	-	SW PURPLE TB2603C, SEMI-GLOSS
7	AWNINGS	SIGNAGE VENDOR	-	BLACK BY THE SIGNAGE VENDOR
8	CORNER TOWER	TAYLOR	BRICK	ROYAL SALISBURY
9	RECESS OF SIDE ENTRY PORTAL	TAYLOR	BRICK	ROYAL SALISBURY
10	HARDIE PANEL	JAMES HARDIE	PANEL SYSTEM SMOOTH	SW PURPLE TB2603C, SEMI-GLOSS
11	METAL PARAPET CAP	-	24GA GALVANIZED	SHERWIN WILLIAMS SW7076 "CYBERSPACE" KYNAR 500 COATING
12	CORNICE TRIM	-	-	SHERWIN WILLIAMS SW7043 "WORLDLY GRAY", SEMI-GLOSS
13	HARDIE PANEL	JAMES HARDIE	PANEL SYSTEM SMOOTH	SHERWIN WILLIAMS SW7076 "CYBERSPACE", SEMI-GLOSS

WEST ELEVATION





SYMBOL	AREA	MANUFACTURER	MATERIAL SPEC.	COLOR
1	FIELD BRICK	GLEN GERY	BRICK	BURN ALMOND
2	ACCENT BRICK	GLEN GERY	BRICK	RED CLIFF
3	DOWNSPOUTS	-	-	SHERWIN WILLIAMS SW7043 "WORLDLY GRAY", SEMI-GLOSS
4	ACCENT BRICK	TAYLOR	BRICK	ROYAL SALISBURY
5	HARDIE TRIM	JAMES HARDIE	HARDIE TRIM 5/4 SMOOTH 1" x 5 1/2"	SHERWIN WILLIAMS SW7076 "CYBERSPACE", SEMI-GLOSS
6	HOLLOW METAL DOOR	-	-	SW PURPLE TB2603C, SEMI-GLOSS
7	AWNINGS	SIGNAGE VENDOR	-	BLACK BY THE SIGNAGE VENDOR
8	CORNER TOWER	TAYLOR	BRICK	ROYAL SALISBURY
9	RECESS OF SIDE ENTRY PORTAL	TAYLOR	BRICK	ROYAL SALISBURY
10	HARDIE PANEL	JAMES HARDIE	PANEL SYSTEM SMOOTH	SW PURPLE TB2603C, SEMI-GLOSS
11	METAL PARAPET CAP	-	24GA GALVANIZED	SHERWIN WILLIAMS SW7076 "CYBERSPACE" KYNAR 500 COATING
12	CORNICE TRIM	-	-	SHERWIN WILLIAMS SW7043 "WORLDLY GRAY", SEMI-GLOSS
13	HARDIE PANEL	JAMES HARDIE	PANEL SYSTEM SMOOTH	SHERWIN WILLIAMS SW7076 "CYBERSPACE", SEMI-GLOSS

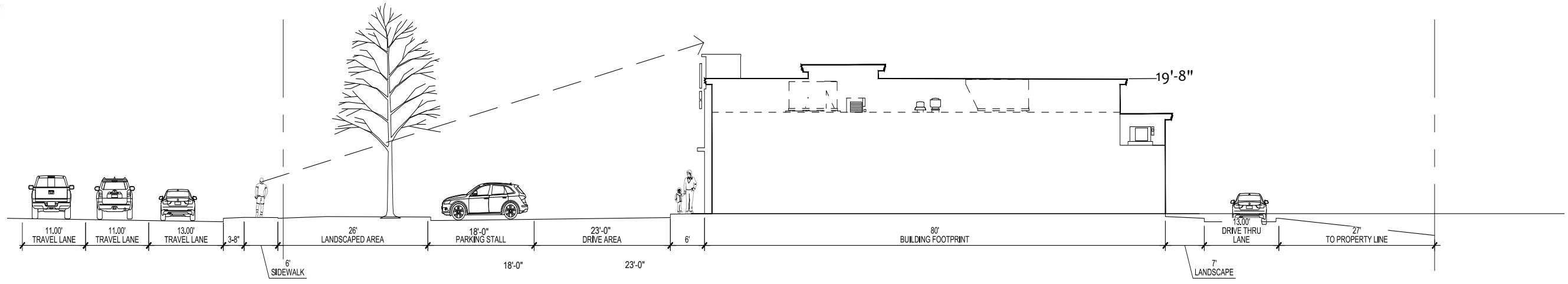
NORTH ELEVATION











SIGHT-LINE DIAGRAM - VIEW PERSPECTIVE SHOWN IS FROM THE EAST SIDE OF THE BUILDING



SIGHT-LINE FOR ROOFTOP EQUIPMENT SCREENING





EXTERIOR ELEVATIONS



FRONT ENTRY / SOUTH ELEVATION





**DYNAMIC**  
ENGINEERING EARTH SURVEY TRAFFIC



EAST ELEVATION



WEST ELEVATION



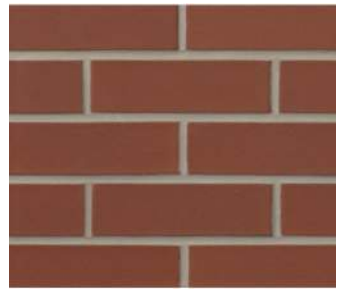


NORTH ELEVATION





B1 - GLEN GERY  
BURNT ALMOND



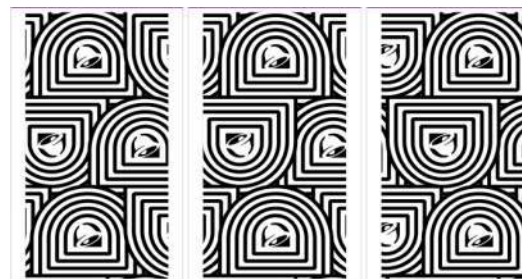
B2 - GLEN GERY  
RED CLIFF



B3 - TAYLOR  
ROYAL SALISBURY



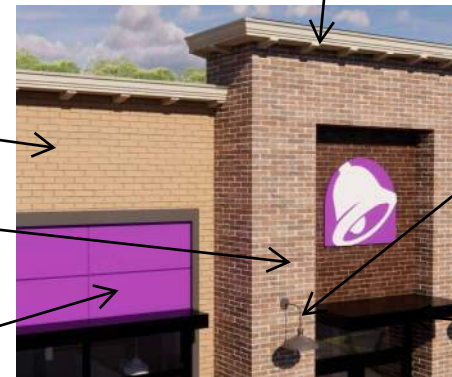
P-1 TACO BELL  
PURPLE  
(PROPRIETARY COLOR)



EXTERIOR ARTWORK PANELS



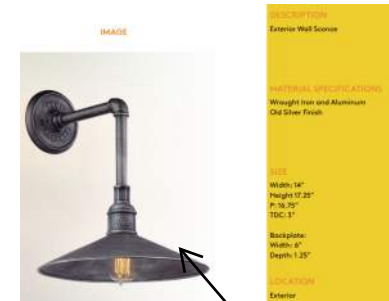
CORNICE DETAIL



B-1

B-2

P-1



EXTERIOR WALL  
SCONCE



METAL EYEBROW  
CANOPY  
6" HIGH

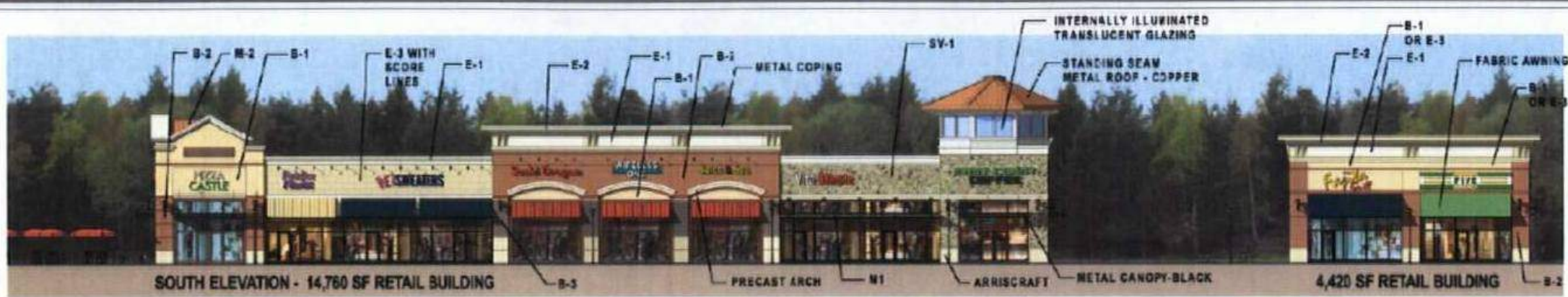


42" BELL  
14" LETTERS  
SIGN AREA: 24.4 SF  
PROPOSED SIGNAGE



PATIO EQUIPMENT





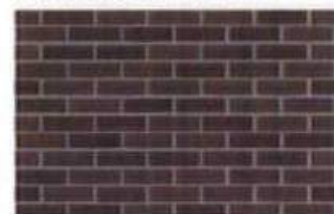
**MATERIAL SELECTION**



B-1 BRICK  
GLEN GERY GRICK - BURNT ALMOND



B-2 BRICK  
GLEN GERY BRICK - RED CLIFF



B-3 BRICK  
TAYLOR - ROYAL SALISBURY

MORTAR - BUFF - LEHIGH #4704

COLORS MAY NOT REPRODUCE ACCURATELY.  
REFER TO SAMPLES PROVIDED.



FABRIC AWNINGS - VARIOUS  
COLORS AND PATTERNS ARE FOR ILLUSTRATION  
ONLY AND ARE SUBJECT TO CHANGE



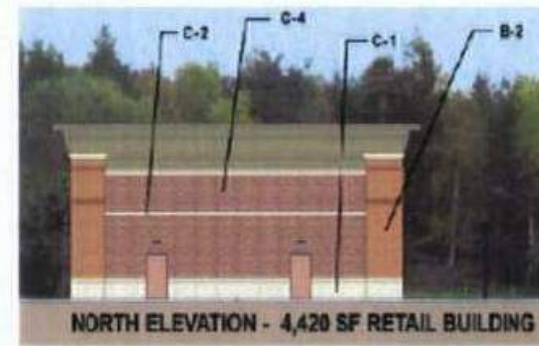
PRECAST CONCRETE - LIMESTONE



ARRISCRAFT RENAISSANCE - WHEAT



WEST ELEVATION - 4,420 SF RETAIL BUILDING



NORTH ELEVATION - 4,420 SF RETAIL BUILDING



E-1 EIFS  
DRYVIT #442 COTTON



E-2 EIFS  
DRYVIT # 109 MANOR WHITE



E-3 EIFS  
DRYVIT # 112 SANDALWOOD BEIGE



C-1 SPLIT FACE CMU  
BETCO SUPREME  
BUFF # 5246



C-2 GROUND FACE CMU  
BETCO SUPREME CMU  
BUFF # 5246



C-3 SPLIT FACE CMU  
BETCO SUPREME  
WILLIAMSBURG RED



C-4 GROUND FACE CMU  
BETCO SUPREME  
WILLIAMSBURG RED



M-1 METAL - GREEN  
COLOR TO MATCH  
DREXMET  
EVERGLADE MOSS



M-2 STANDING  
SEAM METAL ROOF  
COPPER METALLIC



M-3 STOREFRONT/  
CANOPY  
BLACK



M-4 STOREFRONT/COPING  
TO MATCH KAWNEER  
CHAMPAGNE #18



EIFS SCORING DETAIL



SY-1 ELDRADO STONE VENEER  
FIELDSTONE



PROPOSED DUMPSTER GATES & ENCLOSURE TO  
MATCH EXISTING SITE STYLE AND MATERIALS







PATIO FURNITURE

## PRE-ASSEMBLED ALUMINUM RAILING INSTALLATION INSTRUCTIONS

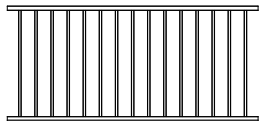
### Items and Tools Needed

#### Parts included

- |                           |   |
|---------------------------|---|
| (1) Pre-assembled panel   | (4) Rail support connectors for 6ft         |
| (2) Rail supports for 6ft | (6) Rail support connectors for 8ft         |
| (3) Rail supports for 8ft | (4) Brackets with hardware                  |
|                           | (Posts and stair rail kits sold separately) |

#### Tools required

- |  |  |                 |
|--|--|-----------------|
| • Drill/power screwdriver                      | • Carpenter's pencil                                 | • Rubber mallet |
| • Miter or circular saw with carbide tip blade | • Adjustable wrench or socket wrench for bolts, etc. | • Tape measure  |
| • Marked speed square                          | • Safety glasses/goggles                             | • Lag screws    |
| • Carpenter's level                            |  |                 |



(1) Pre-assembled Panel



(2 or 3) Rail Supports



(4 or 6) Rail Support Connectors

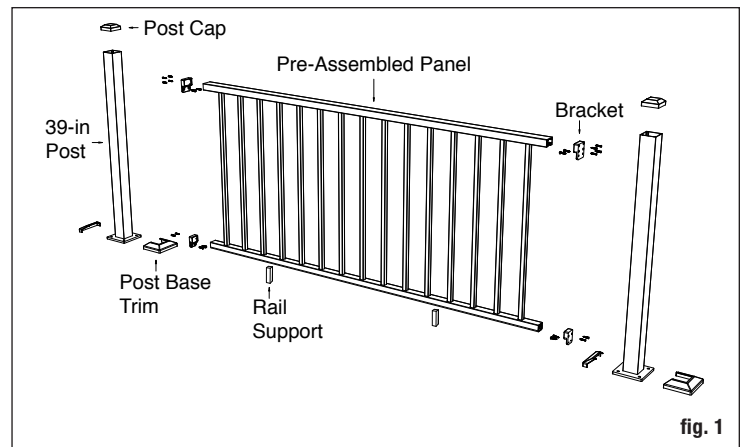
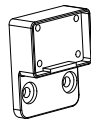


fig. 1



(2) Top Rail Brackets



(2) Bottom Rail Brackets



(12) Metal Screws



(4) Large Screws



(4) Small Screws

### In-Line Railing Installation Instructions

#### Posts Installation

##### Prior to construction:

- Check with your local regulatory agency for special code requirements in your area. Common railing height is 36" or 42". Read instructions completely to get an understanding of how the product goes together and how each piece affects the other.
- Determine the number of railing posts needed for your deck. Post spacing is 6' or 8' on-center. Example: A 12' x 16' deck attached to a building with a 4' access opening on one side will need a total of eight posts. To minimize cutting, use as many full panels as possible.

**Step 1:** Install posts by attaching the aluminum base to the surface of the deck. Position the post so the fastener will go into the floor joist, and make sure the decking is firmly attached to the joist at the location of the posts. Proper structural blocking/framing under the decking material is required when attaching the post to a wood frame deck because decking alone is not approved as structural framing (fig. 2).

**Step 2:** Position the post assembly onto the location where it will attach to the deck. Four 3/8" diameter mounting holes are provided on the base. When the final position is determined, mark the base hole locations. Remove the post assembly and drill 15/64" holes in the marked locations through the decking and into structural blocking (fig. 3).

**Step 3:** Reposition the post assembly over the predrilled holes and insert the fasteners (**NOT INCLUDED**). Secure the base to the deck structure. Make certain the posts are plumb. *Note: Recommend 5/16" x 4" or longer lag screws. If the post requires adjustment, use shims to level post. If the post requires adjustment, use shims to level post.*

**Step 4:** Finish by sliding a post base trim (optional) over each post for a finished look (fig. 4).

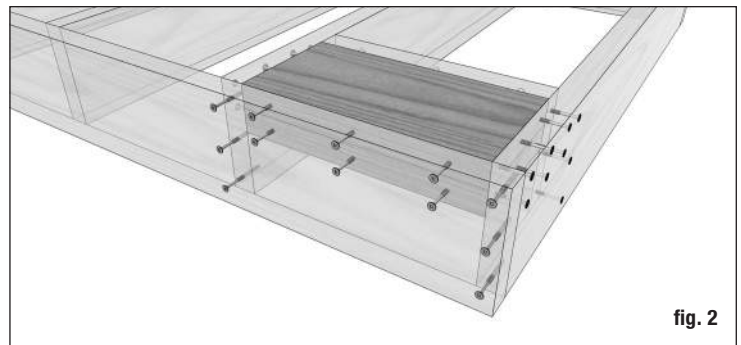


fig. 2

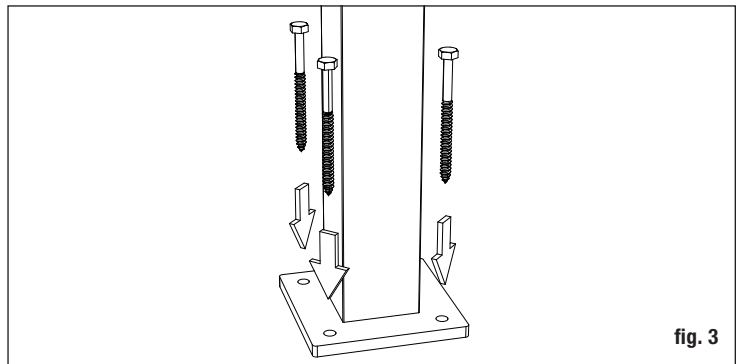


fig. 3

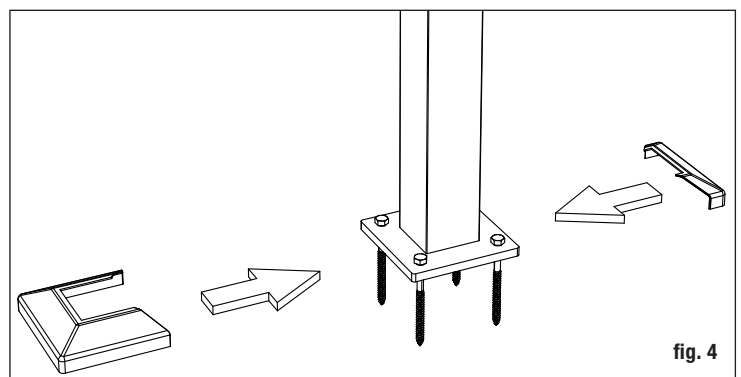


fig. 4

## IMAGE



## DESCRIPTION

Exterior Aluminium Chair

## MATERIAL SPECIFICATIONS

Legs:

Black PowderCoat

Body:

Black PowderCoat slats

## SIZE

H = 28"

W = 22"

D = 20"

## LOCATION

Exterior Patio- if applicable

## OTHER DETAILS

Produced by Taco Bell approved furniture vendor

## BUILDING DESIGN

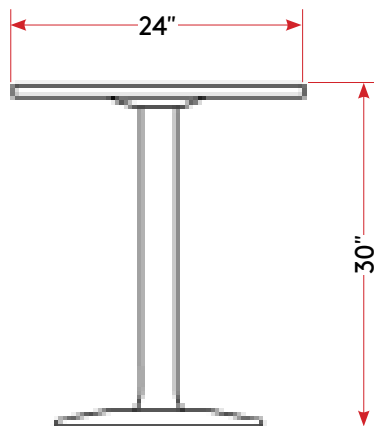
Endeavor New & Remodels

## REVISION DATE

2020.09.23

*Image shown NTS*



**IMAGE****DIMENSIONS****DESCRIPTION**

Exterior Round Table

**MATERIAL SPECIFICATIONS**

Legs:

Black PowderCoat

Body:

Black PowderCoat slats

Plastic wood - look slats

**SIZE**

H = 30"

DIA = 24"

3" Base column

**LOCATION**

Exterior Patio- if applicable

**OTHER DETAILS**

Produced by Taco Bell approved furniture vendor

**BUILDING DESIGN**

Endeavor New &amp; Remodels

**REVISION DATE**

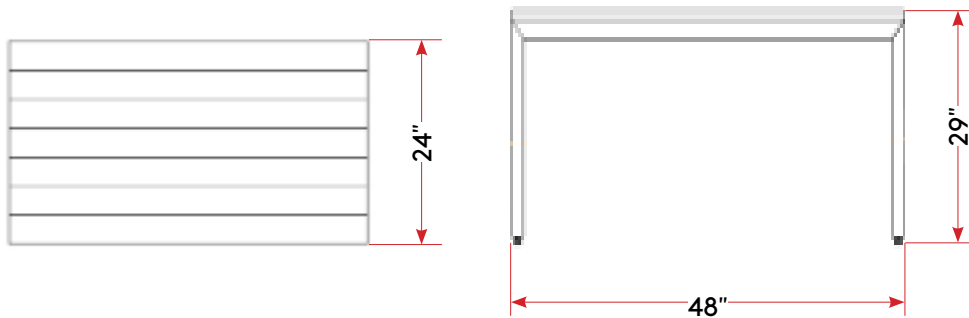
2020.09.23

*Image shown NTS*

### IMAGE



### DIMENSIONS



### DESCRIPTION

Exterior Rectangle Table

### MATERIAL SPECIFICATIONS

Legs:  
Black PowderCoat

Body:  
Black PowderCoat slats

Plastic wood - look slats

### SIZE

H = 29"

W = 48"

D = 24"

### LOCATION

Exterior Patio- if applicable

### OTHER DETAILS

Produced by Taco Bell approved furniture vendor

### BUILDING DESIGN

Endeavor New & Remodels

### REVISION DATE

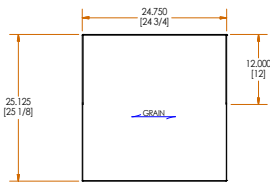
2020.09.23

*Image shown NTS*

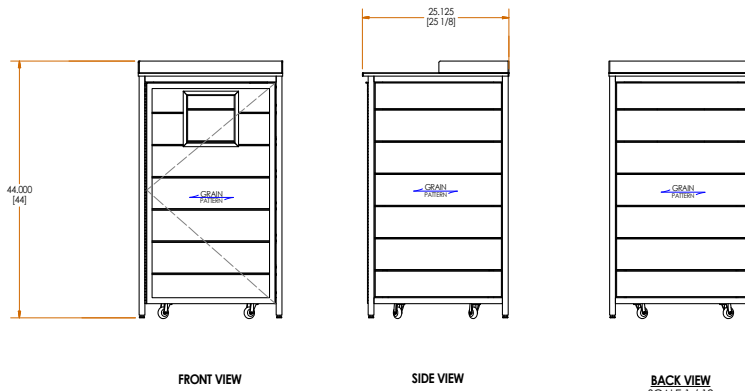
## IMAGE



## DIMENSIONS



TOP VIEW



FRONT VIEW

SIDE VIEW

BACK VIEW  
COURTESY OF TACO BELL

Image shown NTS

## DESCRIPTION

Single-entry trash enclosure  
exterior grade

## MATERIAL SPECIFICATIONS

Legs:  
Black PowderCoat

Body:  
Wood, Deckorators

Top:  
Metal- stainless steel

## SIZE

H = 44"

W = 46.75"

D = 24"

## LOCATION

Exterior

## OTHER DETAILS

Produced by Taco Bell approved  
furniture vendor

## BUILDING DESIGN

Endeavor

## REVISION DATE

2020.08.11

*Parasol platform: Bay Master MAX*

*For Medium Patios*



**Bay Master MAX Classic Square**

**Frame Construction:**

- Satin Anodized Marine Grade finish
- TUUCI' s Patented Independent Bracket Hub System
- Armor Wall Mast
- Stainless Steel Hardware
- Modular Design Allows for Easy Parts Replacement
- Manual lift System
- SKU: UTBBMX10.OSQ

**Canopy Construction:**

- Sunbrella® 100% Solution Dyed Acrylic Fabric – CONCORD 6065
- Ballistic-Reinforced Vent and Pocket Construction
- Market Profile Canopy
- Canopy with Vent
- \*\*5 Year Warranty\*\***

**Sizes Available**

10' Square



## Shade Anchoring System for MAX:

For Bay Master MAX & Ocean Master MAX



### In-Ground Bases

In Ground Security Mount  
New Pour Concrete:



In Ground 9" x 9" Base Plate  
Existing Concrete

---

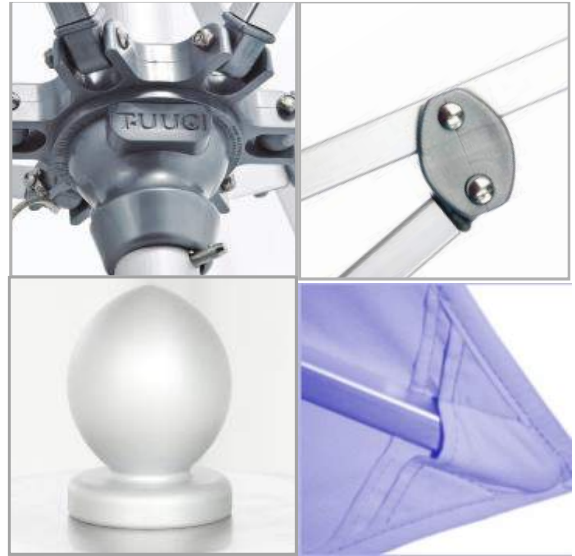
### Above Ground Base

Galvanized Steel Double Stack Plates  
280 lbs. ( For 8'SQ, 10' SQ)  
400 lbs. (For 12' SQ, 8X12' RECT, and 10'X14' REC)



## *Parasol platform: Bay Master Classic*

*For Small Patios*



### **Bay Master Classic Square**

#### **Frame Construction:**

Satin Anodized Marine Grade finish  
TUUCI's Patented Independent Bracket Hub System  
Stainless Steel Hardware  
Modular Design Allows for Easy Parts Replacement  
Armor Wall Mast, 8 ribs  
Manual lift with a pin

#### **Canopy Construction:**

Sunbrella® 100% Solution Dyed Acrylic Fabric – CONCORD 6065  
Ballistic-Reinforced Vent and Pocket Construction  
Market Profile Canopy  
Canopy with Vent

### **Size:**

8.5' Square

***\*\*3 Year Warranty\*\****



Shade Anchoring System for Classic:

(1.5" center pole)



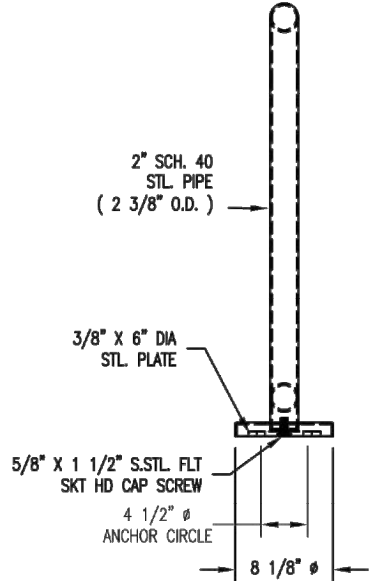
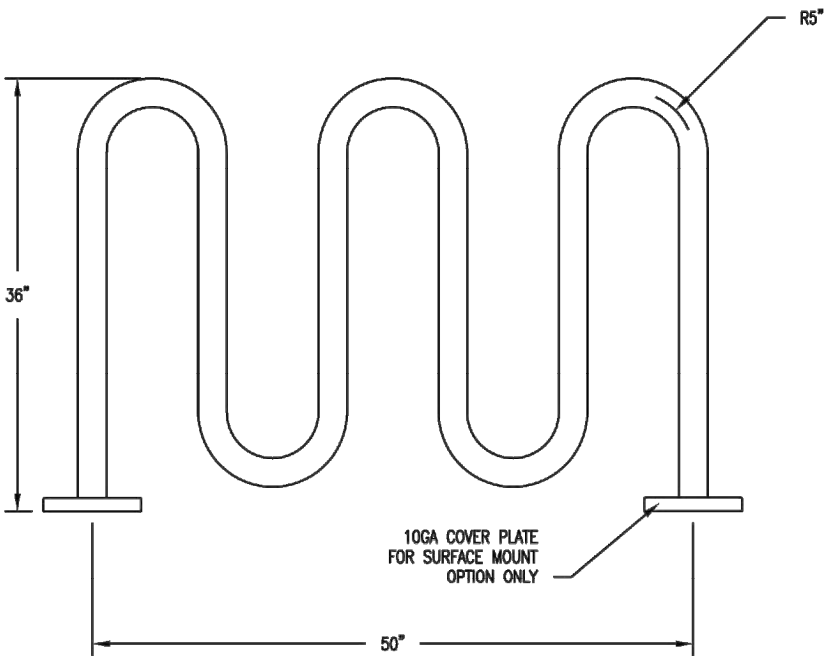
**In Ground BASE:**

In Ground Flush Mount  
Concrete



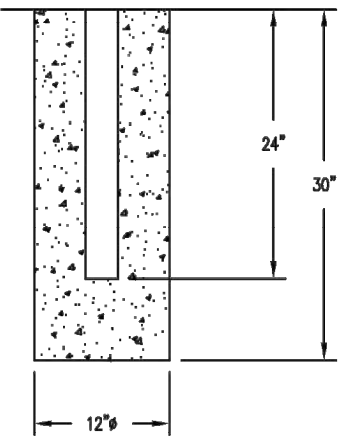
**Above Ground BASE:**

Galvanized Steel Plate  
140 lbs. (For the 8.5' SQ)

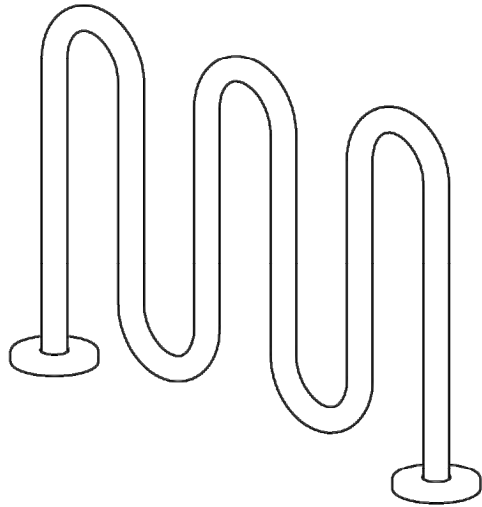


\*  S-2 SURFACE MOUNT \*

(SEE NOTES)



S-1 EMBEDMENT



FINISH OPTIONS

- HOT DIPPED GALVANIZED ( VISIBLE VENT HOLES )
- COATED W/ ZINC RICH EPOXY THEN FINISHED W/ POLYESTER POWDER COATING.

NOTES:

- 1.) 1/2" X 3 3/4" EXPANSION ANCHOR BOLTS PROVIDED FOR OPTION S-2.
  - 2.) PLUGS PROVIDED FOR VISIBLE VENT HOLES. ( HOT DIPPED GALV. ONLY )
- \* UPCHARGE APPLIES TO SUPPORT OPTION - CONSULT YOUR LOCAL REPRESENTATIVE.

**DuMor, inc.**  
P.O. Box 142 Mifflintown, PA 17059-0142

SCALE :	NONE
DATE DRAWN :	1/6/11
DRAWN BY :	JSB
DATE REV. :	
REV. BY :	

TITLE :		BIKE RACK	
REV.	DRAWING NUMBER		
A	130-30		
		SHEET	1 OF 2



**ABTB Mid-Atlantic, LLC**

**STATEMENT OF INTENT**

**January 18, 2024**

Please accept the following as a Statement of Intent in support of the submitted Major Certificate of Appropriateness application to allow for the development of 10120 Fairfax Boulevard with an approximately 2,256 square foot restaurant with a drive-through window/facility. This Statement of Intent is submitted in conjunction with the Certificate of Appropriateness design package, consisting of thirty-one (31) sheets, and other submitted supporting materials. The contents of this Statement of Intent address the approval considerations for certificates of appropriateness, as set forth in Section 6.5.7.A of the City of Fairfax Zoning Ordinance (the “Zoning Ordinance”).

ABTB Mid-Atlantic, LLC (the “Applicant”) is the contract lessee of an approximately 0.66 acre property located in the City of Fairfax. The property consists of one (1) tax parcel identified among the City of Fairfax’s tax assessment records as 47-4-23-000-D (the “Subject Property”). The Subject Property is currently a vacant pad site that is part of the Boulevard Marketplace Shopping Center (the “Shopping Center”). Zoned to the CR (Commercial Retail) District, the Subject Property is included in a Generalized Development Plan (“GDP”) that was approved in conjunction with a rezoning in 2008, allowing a total of 17,300 square feet of commercial space, including a 4,300 square foot bank building with drive-through on the Subject Property. In 2011, the Shopping Center was subdivided into two separate parcels, including the Subject Property and the adjacent parcel identified as 47-4-23-000-A (“Parcel A”). In 2017, City Council approved a GDP amendment, which converted the previously approved drive-through bank on the Subject Property to an approximately 5,100 square foot retail building. To date, the approved retail building has not been constructed, and the Subject Property remains vacant.

**PROPOSED DEVELOPMENT**

The Applicant proposes to redevelop the Subject Property with a drive-through Taco Bell restaurant. No modifications or improvements are proposed to Parcel A nor its existing buildings. The proposed drive-through restaurant is a commercial use that is consistent with the recommendations of the Comprehensive Plan and compatible with the commercial character of the surrounding area. The proposed restaurant will consist of an approximately 2,256 square-foot, one story building with a proposed parking area oriented towards the rear of the Subject Property. The proposed drive-through lane will wrap around the new building and proposed parking area. In addition, bicycle parking and a small outdoor dining patio area will be provided proximate to the restaurant’s main entrance. As shown on the submitted materials, the proposed restaurant building will be architecturally compatible with the adjacent buildings in the Shopping Center and will be constructed of high-quality materials.

## CITY OF FAIRFAX DESIGN GUIDELINES

As illustrated in the submitted materials, the overall design of the proposed development is consistent with the City of Fairfax Design Guidelines. The proposed development is characterized by the installation of attractive architecture, the provision of quality landscaping, and a reduction of impervious surface. To ensure compatibility with the existing design of the Shopping Center, the proposed restaurant will include the use of masonry and cornice details that are consistent with the materials of the two existing buildings on Parcel A. Building materials may include, but are not limited to brick, Hardie cement board panels, E.I.F.S, and other quality materials. The proposed restaurant building will be a maximum height of twenty-five (25) feet. As such, the mass, scale, height, and architectural style of the proposed building will be compatible and proportionate to the remainder of the Shopping Center.

Quality landscaping is also proposed, including a mix of shade, ornamental, and evergreen trees in combination with evergreen and deciduous shrubs as well as ground cover, perennials, and ornamental grasses along the perimeter of the parking lot and the drive-through lane. The proposed landscaping will provide more vegetation on-site compatible to what is provided on the parcels adjacent to the Subject Property. In addition to the proposed landscaping, the Applicant is proposing to retain all of the existing street trees and other landscaping along Fairfax Boulevard. In the end, the proposed development will result in increased open space and an overall decrease of impervious surface.

The proposed site lighting is shown on Sheets 7 and 8 of the design package. All proposed site lighting will comply with Zoning Ordinance requirements. Lighting surrounding the building will consist of LED lighting on 25'-0" bronze posts and LED fixtures, consistent with the Zoning Ordinance. All light fixtures will be located and aimed to minimize glare and stray light trespassing across lot lines and into the public right-of-way. All of the existing street lights along Fairfax Boulevard will remain. Overall, the Applicant's lighting plan will illuminate the proposed parking lot and pedestrian pathways to provide safe vehicular and pedestrian circulation as well as minimize pedestrian/vehicular conflicts.

As to additional appurtenances associated with the proposed development, the Applicant has included an enclosed dumpster that will be located in the northern area of the proposed parking lot on the Subject Property. The proposed dumpsters will be screened by a combination of evergreen trees, evergreen shrubs, and a shade tree surrounding a brick enclosure with painted solid metal gates to match the building in order to minimize visual impact. Additionally, bicycle parking is proposed in accordance with the Zoning Ordinance requirements. Lastly, any rooftop mechanical equipment installed on the roof of the proposed restaurant building will be screened.

The proposed improvements will encourage and foster enhanced pedestrian connectivity within the site as well as with the surrounding community. To accommodate adequate pedestrian circulation, the Applicant will incorporate a 6-foot wide sidewalk between the proposed building and parking areas. In addition, the Applicant proposes to maintain an existing 8-foot wide trail along the western property line that connects to the George Snyder Trail to the north of the site. A 3.5-wide landscape strip is proposed between the drive-through lane and the 8-foot wide trail as shown on the submitted materials of the design package to provide an additional vegetated buffer

between pedestrians and vehicles in the drive-through lane. These new sidewalks and connections will contribute to the enhancement of the pedestrian experience along this stretch of the Fairfax Boulevard corridor.

In conclusion, the Applicant's proposal presents an opportunity to redevelop and activate an underutilized parcel with a vibrant, high-quality community-serving use that advances the Comprehensive Plan's objectives of enhancing commercial activities along the Fairfax Boulevard commercial corridor. The proposed site design consists of high-quality architecture, landscaping, and pedestrian improvements that are consistent with the City's Design Guidelines and will enhance the quality of one of the City's primary commercial corridors. The Applicant is eager to bring the first Taco Bell to the City and is committed to ensuring that the proposed development fits into the fabric of Fairfax.

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May 17, 2024

City of Fairfax  
Department of Community Development & Planning  
10455 Armstrong Street, Suite 207  
Fairfax, VA 22030

Attn: Mr. Albert Frederick  
Senior Planner

**RE: Fairfax Boulevard Taco Bell/  
10120 Fairfax Boulevard  
City of Fairfax, VA  
Rezoning Application  
DEC# 3486 22-01538  
Project #: Z-24-00085 (1<sup>st</sup> Submission)**

Dear Mr. Frederick,

On behalf of the Applicant, ABTB Mid-Atlantic LLC, below please find the Applicant's responses to City staff's comment letter dated April 5, 2024 in regard to the above-referenced project. The responses have been provided in bold for your convenience and reference the revised General Development Plan/Special Use Permit Plat dated January 18, 2024 as amended through May 17, 2024 (the "Revised GDPA/SUP Plat"), Statement of Justification dated May 17, 2024 (the "Revised SOJ"), and/or other associated application materials, as applicable.

**Community Development and Planning:**

A1. *Provide a table with uses, square footage, and required parking for the overall shopping center including the subject site for Taco Bell.*

**A table and overall plan view for the Boulevard Marketplace Shopping Center ("Shopping Center") has been provided. Please see Sheet 4 of the Revised GDPA/SUP Plat.**

A2. *Parking lot islands are required for every 10 spaces. (Section 4.5.7 – Parking Lot Landscaping).*

**Acknowledged. To provide pedestrian connectivity from the existing sidewalk along Fairfax Boulevard to the entrance of the proposed restaurant, a new sidewalk will be installed that will connect to a proposed parking lot island and crosswalk as shown on Sheet 3 of the Revised GDPA/SUP Plat. The Applicant will also improve this existing row of parking in accordance with current Code requirements of Section 4.5.7 of the Zoning Ordinance.**

A3. *What is the seating capacity for Taco Bell?*

**Total interior is 40 seats. Total exterior planned capacity is 10 seats.**

A4. *Provide hours of operation.*

**The anticipated hours of operation are Sunday to Thursday from 8:00 AM to 1:00 AM, and Friday to Saturday from 8:00 AM to 2:00 AM.**

A5. *Discuss the number of full-time and part-time employees anticipated for the site.*

**For a restaurant of this size, the anticipated number of employees is 30 total, comprising of 18 full-time employees and 12 part-time employees.**

A6. *Show building setback on the west side of the proposed building.*

**The building setback on the west side of the proposed building is shown on Sheet 3 of the Revised GDPA/SUP Plat.**

A7. *Show distance from the proposed building to the Resource Protection Area (RPA).*

**The closest distance dimension from proposed building to RPA is now shown on the plan. See Sheet 3 of the Revised GDPA/SUP Plat.**

A8. *The applicant should consider the impacts to the RPA by providing certain plantings or other ground cover, and a no mow zone.*

**Acknowledged. The previous pad construction provided plantings along the RPA for this purpose. These existing conditions will not be modified or impacted by the proposed construction.**

A9. *Verify the lot coverage proposed for Parcel D. Provide lot coverage and building coverage for Taco Bell site and the overall shopping center.*

**Lot coverage is provided for both Parcel D and the overall Shopping Center. See Sheet 4 of the Revised GDPA/SUP Plat.**

A10. *The trail on west side of lot reads as 6-foot wide, not 8 feet on the site. The trail is in the 10-foot side yard setback. The measurement should be measured from the property line. Refer to the Multimodal Plan for guidance on pedestrian connections. A Special Exception may be required.*

**The trail measurement has been confirmed to be 6 feet and has been revised on the GDPA/SUP Plat accordingly. The existing trail is within an existing 12-ft trail easement, where the remaining 6 feet are located adjacent to the property line, preventing the ability to adhere to the 10-ft side yard requirements.**

- A11. *The GDP amendment is for the entire site and the multimodal section of the comprehensive plan calls for a 10-foot shared use path along Fairfax Boulevard.*

**The Applicant proposes to maintain the existing 6-foot-wide sidewalk along Fairfax Boulevard. No physical changes or site improvements are proposed to Parcel A, including this sidewalk. Thus, to widen only the portion of this sidewalk that is located on the Property to 10 feet would not be practical, as it would leave the remainder of the sidewalk that is on Parcel A's frontage at the existing 6-feet in width.**

**Alternatively, the Applicant is willing to consider providing an easement in this area of the Property to accommodate a future widening of this sidewalk by others in accordance with Comprehensive Plan recommendations should Parcel A choose to redevelop at a later date.**

- A12. *There are twelve parking spaces near Fairfax Boulevard that need to be adjusted to meet the ordinance requirement stated in Comment A2. The applicant should consider eliminating a space or two to provide a pedestrian connection to the front of the store and provide a sidewalk out to Fairfax Boulevard.*

**See Applicant's response to Comment A2. This has been incorporated into the design.**

- A13. *Provide a table with traffic counts based on existing uses for the shopping center, including the proposed use and the previously approved uses for the site.*

**Traffic counts will be provided with the TIS materials under separate cover.**

- A14. *Provide details for screening of the dumpster (Section 4.5.8.G of the Zoning Ordinance).*

**The general appearance of the proposed dumpster and dumpster gate is designed to match the aesthetic and materials of the other enclosures within the larger Shopping Center. The proposed design has been included with the Board of Architectural Review ("BAR") package and architectural plans. Additionally, the enclosure will be screened partially by proposed vegetation as detailed on the Conceptual Landscape Plan. See Sheet 7 of the Revised GDPA/SUP Plat.**

- A15. *The concrete pad under the dumpsters doesn't appear to meet the requirements of Section 4.12, which states "If dumpsters are used for refuse disposal, then each dumpster shall be located on a concrete pad with minimum dimensions of 20 feet by 12 feet and screened in accordance with the requirements contained in § 4.5. Refuse disposal areas shall be located so that they are accessible by a disposal truck without impeding traffic or encroaching upon required parking spaces." The dumpsters shown on Sheet 3 are measured at 10'x24'. The 24' in width is fine for what appears to be two dumpsters (with something smaller in between them), but the concrete pad should extend out another 10' into the parking lot to 20' total in length. Each dumpster needs to meet the 12'x20' requirement.*

**The size of the overall dumpster pad has been increased to meet the referenced requirement.**

A16. *Provide tree canopy coverage, including the number of canopy, understory and shrubs required and provided. Demonstrate compliance with street tree provisions of the ordinance. Also, appears that existing trees are in the utility easements, which may preclude meeting the street tree requirements. Can trees be provided in the parking lot area? A Special Exception may be required.*

**See the Conceptual Landscape Plan for the current design proposed. The intent of the design was to not impact any of the established trees or vegetation on-site. A tree survey has also been provided with additional information on the existing perimeter trees.**

A17. *Signs are subject to Section 4.6 of the Zoning Ordinance. If you have questions, please contact Joe Eisenberg, [joesph.eisenberg@fairfaxva.gov](mailto:joesph.eisenberg@fairfaxva.gov) to discuss Section 4.6 (Signs) and the application process.*

**Acknowledged. No response needed.**

A18. *Provide a fiscal impact statement with the next submission.*

**The Applicant is working with Eric Forman with the City to determine what information is required to prepare a fiscal impact statement applicable to this proposed use.**

A19. *Contact person listed is Robert Brant, should be changed to Kathryn Taylor.*

**Contact person has been changed accordingly. See Sheet 1 of the Revised GDPA/SUP Plat.**

#### **Community Development and Planning/Architectural Review:**

B1. *The Board of Architectural Review held a post-submission work session with the applicant on March 6, 2024.*

**Acknowledged. No response required.**

#### **Community Development and Planning/Urban Forester**

C1. *A tree conservation plan is required. This submission is incomplete (Code Section 4.5.4). Use the updated 2024 Site Plan Package Checklist on the City's website (<https://www.fairfaxva.gov/government/community-development-planning/zoning>) to meet outstanding requirements.*

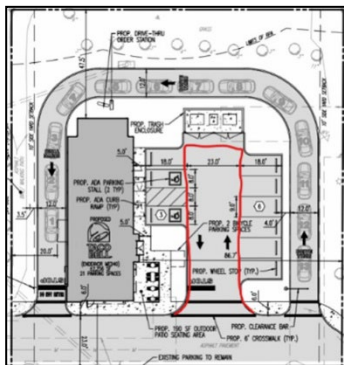
**Acknowledged. At this stage in the development review process, a tree survey has been provided, along with an ESC/construction management plan that confirms the LOD will pose no impacts to established trees/vegetation on the Property. A tree conservation plan will be provided at the time of site plan review as required.**

C2. *Preservation measures and details will be required with the site plan submission.*

**Acknowledged – see response above.**

## Code Administration

- D1. *The drive lane down the center of the parking lot will need to be designated a Fire Lane (See example in photo below).*



**In accordance with a meeting with Chief Sites and the Applicant's team on May 9, 2024, a fire marshal plan has been provided noting the limits of the fire lane. The limits of the fire lane internal to the parking area will run along the limits of the front curb but will end at the first handicap parking stall. See Sheet 14 of the Revised GDPA/SUP Plat.**

- D2. *Will need fire apparatus turning movements (both directions) into the fire lane. Tower Ladder 403 specifications below.*

*Pierce Velocity Chassis - Tower Ladder 403 (100' Aerial Platform)*

*Overall Length: 561.25" (46' - 9.25")*

*Overall Height: 146" (12' - 2")*

*Overall Width: 10' - 2"*

*Turning Radii:*

*Inside Turn: 20' - 9"*

*Curb to Curb: 37' - 4"*

*Wall to Wall: 45' - 0"*

*Wheelbase: 257" (21' - 5")*

*Inside Cramp Angle: 45 degrees*

*Tread Width: 17.7"*

**Truck turning from both directions to be provided in a subsequent submittal.**

- D3. *Given the drive lane in the parking area will be a fire lane, and the service road in front of the property is already (and will remain) a fire lane, the applicant needs to share where delivery trucks will park.*

**Deliveries will be designated for the rear of the internal parking area. The fire lane is proposed to end at the first handicap stall, leaving room for delivery operations without impacting the fire lane. It is anticipated that delivery trucks will back up into the restaurant parking area parallel to the side of the building, where deliveries typically occur twice a week at approximately 4:00 AM.**



- D4. *Will the business have an exterior carbon dioxide tank? If so, please show where this will be located. If located anywhere near motor vehicles it will need vehicle impact protection as well.*

**The CO2 tank will be located inside the proposed restaurant.**

### **Historic Resources**

- E1. *No comments.*

### **Public Works/Engineering**

- F1. *Sheet 3 - Stormwater management narrative states that this project is grandfathered under the Part II C technical criteria, which were used to calculate the initial stormwater treatment requirements in 2011/2012.*

*Per 9VAC25-870-48.C, the grandfathered activity shall only remain such for one additional state permit cycle, which ended June 30, 2019. After such time, portions of the project not under construction become subject to new technical criteria (Part II B).*

*Please run a VRRM under Part II B criteria to determine if BMP installed in 2012 will generate enough credit to meet the Part II B technical criteria. Rerun landcover for the entire site. The calculations must demonstrate that requirements of 9VAC25-870-65 and 9VAC25-870-66 have been met. Provide calculations for the 1-yr, 2-yr, and 10-yr design storms.*

**Acknowledged. The Applicant has removed the portion of the stormwater narrative related to any grandfathering of the site. The analysis using the most up-to-date VRRM is in progress, and updated spreadsheets with the site information demonstrating that the existing device still meets the updated regulations will be provided with the subsequent submittal and site plan.**

- F2. *Show that the proposed grading can convey the 100-year overland relief without impact to surrounding structures, including providing a map showing 100-yr flow with directional arrows. This only applies to Parcel D.*

**The 100-yr overland relief path is shown on the preliminary grading/utility plan on Sheet 6 of the Revised GDPA/SUP Plat.**

- F3. *Provide Erosion and Sediment Control plan, including construction entrance as this must traverse a neighboring commercial property.*

**An ESC plan is now provided. The main drive aisle on the Property is existing and will remain open throughout construction. Please see the ESC plan included for additional information on the intended construction entry/exit point. See Sheet 5 of the GDPA/SUP Plat.**

- F4. *Provide a construction management plan that includes pedestrian and vehicular traffic control. Erosion and Sediment Control plan must include any safety fence necessary to separate the construction area from publicly accessible areas.*

**A construction management plan and details have been incorporated into the ESC plan. See Sheet 5 of the Revised GDPA/SUP Plat.**

F5. *Show all existing and proposed maintenance easements.*

**Maintenance easement limits will be included in the plan set once finalized.**

### **Public Works/Transportation**

G1. *Add sight distance exhibit at drive-thru exit – appears to be very limited visibility due to the building placement, both sidewalks and the access lane.*

**A sight distance exhibit is shown for this location, illustrating the clear obstruction triangle available for the driver. See Sheet 11.**

G2. *Add sight distance exhibit at parking lot exit – appears to be somewhat limited due to building, fencing, and patio seating.*

**Sight distance exhibit is shown for this location, illustrating the clear obstruction triangle available for the driver. See Sheet 11 of the Revised GDPA/SUP Plat.**

G3. *Add signage and striping sheet. Add stop signs in addition to stop bars, add wayfinding signs to drive-thru as appropriate. Confirm with the Fire Marshal what fire lane signage and striping is required.*

**Signage and striping sheet has been provided. Details will be finalized with direction from both the operator and overall Shopping Center. Any fire marshal specific details are shown/provided on the fire marshal plan. See Sheet 14 of the Revised GDPA/SUP Plat.**

G4. *Add turning movement exhibit and loading space information for delivery vehicles.*

**Additional turning movement information and routing is provided. See Sheets 8 and 9.**

G5. *Trash truck turning movement shows encroachment into opposing lanes.*

**Turning movement has been adjusted accordingly. The routing illustrates that dumpsters on both sides of the pad are accessible with little impact to the overall movement.**

G6. *Note whether there is a cross-parcel access easement. If not, drivers should enter through the western entrance.*

**There is a cross parcel access easement found in Deed Book 24317 Page 912. The Applicant will provide a copy of same upon request.**

G7. *Note whether there is a public access easement for the trail. If not, is one required?*

**There is a public access easement for the trail found in Deed Book 24506 Page 1040.**

G8. *Shrubs between bike path and drive-thru aisle help provide a buffer. Ensure property owner regularly prunes/maintains these shrubs to avoid encroachment on path.*

**A maintenance plan will be incorporated into the final approvals between the property owner and the Applicant as the contract lessee.**

- G9. *Confirm width of bike path and update if needed. From aerial and plan measurements, it appears to be 6' wide, not 8'. Consider widening it if possible.*

**The path is 6-ft wide, and the Applicant does not propose to modify the width. This is consistent with the existing trail easement referenced in Comment A10. In addition, the Applicant is not able to widen the existing trail because the additional width is needed to accommodate a proposed vegetated buffer between the drive-through lane and this pathway.**

- G10. *Wheel stops are a good addition to minimize overhang/encroachment on sidewalk and drive-thru lane. However, this effectively limits the parking stall to 16', not 18'. Where will larger vehicles park if they cannot fit into these spaces?*

**The Applicant anticipates that larger vehicles will park in the existing spaces that front Fairfax Boulevard if additional room is needed.**

- G11. *Confirm ADA curb ramps will be added at all crossings. The eastern side of the drive-thru entrance appears to be an odd angle – ensure ADA compliance can be met.*

**ADA ramps have been added at all corners and crossings where applicable with appropriate detectable warning surfaces integrated. See Sheet 3 of the Revised GDPA/SUP Plat.**

- G12. *Confirm whether an ADA van space is required.*

**An ADA van space is required. The proposed parking layout can accommodate a van space.**

- G13. *Confirm that entrances are ADA compliant.*

**The Applicant has confirmed that the proposed entrances are ADA compliant. In addition, minimum widths and landing areas are provided per ADA standards.**

- G14. *The lighting plan shows no light by the front (south) entrance – entrances should be well-lit.*

**The lighting plan shown only shows the lighting levels per the proposed lighting locations for the drive through and internal parking directly next to the building. Lighting levels for the existing poles that light the existing parking along Fairfax Boulevard are not illustrated on the lighting levels. Those poles are to remain and currently provide sufficient lighting for the entrances to the Shopping Center. See Sheet 12 of the Revised GDPA/SUP Plat.**

- G15. *The Statement of Justification mentions bicycle parking, but the location, number, and dimensions of the bicycle parking are not indicated on the plans.*

**The proposed two bicycle spaces are shown and labeled. They are provided within a concrete pad just off of the main building sidewalk. See Sheet 3 of the Revised GDPA/SUP Plat.**

G16. *Recommend widening sidewalk to 10 feet and landscape strip to 5-foot along Fairfax Blvd frontage per Comprehensive Plan guidance.*

**See Applicant's Response to Comment A11.**

G17. *The Traffic Impact Study should include an estimate of the total number of daily trips.*

**Acknowledged. The TIS has been updated accordingly.**

G18. *The Traffic Impact Study should show and compare to the approved parcel use (bank).*

**Acknowledged. The TIS has been updated accordingly.**

G19. *Needs to add some multimodal details and correct a few minor errors they have in the text.*

- a. *Correct references to road ownership where needed (e.g., Fairfax Boulevard is owned/maintained by City, not VDOT).*
- b. *Correct signal phasing description where needed (e.g., 6-phase signals described as 3-phase)*
- c. *Existing bike facilities: note new bikeshare station on Fair Woods Parkway.*
- d. *Existing transit facilities: note that all CUE routes serve this location ("2" routes are westbound, "1" routes are eastbound).*
- e. *Update all references to "Plantation Parkway" to "Fair Woods Parkway" (noticed a few locations in the appendices).*

**Acknowledged. The TIS has been updated accordingly.**

## **Police**

H1. *The Police Department's comments stem from our department's direct experiences with past criminal activities and our comprehensive understanding of on-the-ground realities of similar structures. Furthermore, our feedback is deeply rooted in the principles of Crime Prevention Through Environmental Design (CPTED). Our feedback for Fairfax Boulevard Taco Bell includes:*

a. *Surveillance:*

1. *The City of Fairfax has partnered with Flock Camera systems (19 cameras on streets throughout) for over watch. If businesses use the same camera systems, the PD will have immediate access – through permissions.*

**The Applicant provides interior and exterior camera systems for the premises. The provider commonly used is Dunbar.**

b. *Access Control:*

1. *Ensure access to private areas of the business is restricted with controlled entrances.*

**The proposed restaurant will have a public front entrance to the building only. Any service room and/or emergency doors are exit only and cannot be opened from the exterior. A proposed access door to the back-of-house is restricted, including signage and locks.**

2. *Business restrooms along Fairfax Boulevard are often used by individuals who are not patrons of the establishment. Proper design, signage and maintenance will help.*

**This has been taken into consideration. Restrooms typically have proper signage, and the restroom hallway is visible from the front counter to monitor access.**

c. *Property Controls:*

1. *The City of Fairfax Police Department enforces No Trespassing violations often. Ensure notices are posted identifying this private property.*

**A “Private Property/No Trespassing” sign will be provided.**

2. *There is a blind corner separating vehicles exiting the drive-thru from pedestrians exiting the front door and making a right. The large brick wall creates a visible blind spot which could be dangerous for pedestrians crossing the drive-thru exit lane. It appears there is going to be a “STOP” sign painted on the asphalt, but nothing Acknowledged indicating to the pedestrian of an oncoming car. It is strongly recommended to add safety features (signage or physical barriers) at this intersection.*

**Additional pedestrian signage and striped crosswalks will be added to ensure safe customer circulation.**

d. *Maintenance:*

1. *Ensure the design allows for easy upkeep and maintenance. Unmaintained grounds are an invitation to criminal activity.*

**The entire site is landscaped and will be properly maintained. Restaurant staff makes daily inspections of the premises to ensure no criminal or unwanted activity takes place.**

e. *Landscape:*

1. *Landscape elements such as loose rocks, bricks, or any other heavy, easily moved items that could potentially be used to damage or breach retail windows should not be permitted. This will reduce the risk of opportunistic crime.*

**Proposed landscaping on this site will include irrigated sod and planters containing decorative mulch and will not include loose rock or any other heavy, easily moveable items.**

**Real Estate**

11. *Certified Plat: Deed Book and Page Number references shown on the plat indicate the legal instrument where the parcels were created. Title ownership for Parcel D is located at Deed Book 21756 Page 821 and for Parcel A Deed Book 27683 Page 0311.*

**Acknowledged.**



**Revenue**

J1. *SCC Boulevard Marketplace LLC has met the filing requirements of this office.*

**Acknowledged.**

J2. *JDC Boulevard will need a business license when they rent to Taco Bell.*

**Acknowledged.**

J3. *A general contractor will need a business license when the job is awarded.*

**Acknowledged.**

J4. *Taco Bell will need a business license when they open.*

**Acknowledged.**

**Treasurer**

K1. *There is one tenant (10140 Fairfax Boulevard) with outstanding taxes; no other tenants have outstanding taxes.*

**Acknowledged.**

**Utilities**

L1. *The property is served by Fairfax Water.*

**Acknowledged.**

L2. *Adequate domestic water service is available from an existing on-site 6-inch water main. Please see the enclosed water system map.*

**Acknowledged. The site will utilize the existing pad connection with a minor extension to accommodate the connection location for the proposed restaurant building.**

L3. *Depending upon the configuration of any proposed on-site water mains, additional water main extensions may be necessary to satisfy fire flow requirements and accommodate water quality concerns.*

**No additional on-site main is proposed.**

L4. *If you have any questions regarding this information, please contact Ross Stilling, P.E., Chief Site Plan Review at 703-289-6385.*

**Acknowledged.**

Should you have any questions, comments or require additional information, please do not hesitate to contact our office.

Sincerely,

**DYNAMIC ENGINEERING CONSULTANTS, PC**

A handwritten signature in black ink, appearing to read 'Connor McManus', with a long horizontal flourish extending to the right.

Connor McManus, PE  
Branch Manager